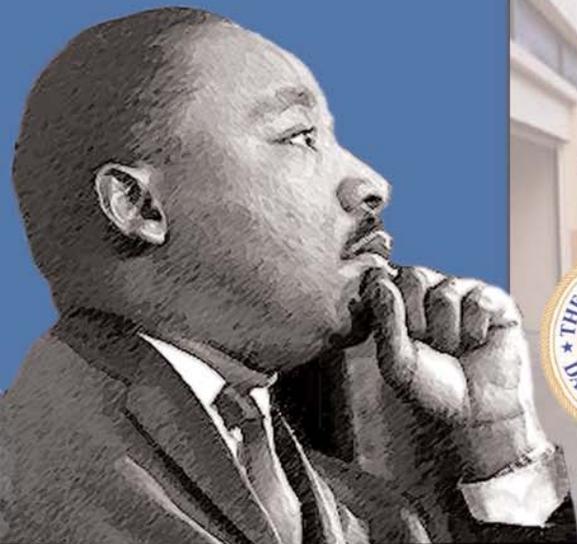


DR. MARTIN LUTHER KING, JR. BOULEVARD STREETScape BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS



Kimley-Horn and Associates, Inc. U R G URBAN RESOURCE GROUP

**FINAL DRAFT SUBMITTAL**

Dr. Martin Luther King, Jr. Boulevard

**STREETSCAPE BEAUTIFICATION  
MASTER PLAN AND FACADE  
STANDARDS**

Prepared for the City of Miami by  
Urban Resource Group,  
a division of  
Kimley - Horn and Associates, Inc.  
June, 2005

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

## City of Miami

MAYOR - MANNY DIAZ

COMMISSIONER ANGEL GONZALEZ - DISTRICT 1

COMMISSIONER JOHNNY L. WINTON - DISTRICT 2

COMMISSIONER JOE SANCHEZ - DISTRICT 3

COMMISSIONER REGALADO - DISTRICT 4

COMMISSIONER JEFFREY L. ALLEN - DISTRICT 5

CITY MANAGER - JOE ARIOLA

Planning Department

Economic Development Department

Community Development Department

444 SW 2nd Avenue

Miami, FL 33130

P.O. Box 330708

## Miami - Dade County

MAYOR - CARLOS ALVAREZ

COMMISSIONER BARBARA J. JORDAN - DISTRICT 1

COMMISSIONER DORRIN D. ROLLE - DISTRICT 2

COMMISSIONER BARBARA CAREY-SHULER - DISTRICT 3

COMMISSIONER SALLY A. HEYMAN - DISTRICT 4

COMMISSIONER BRUNO A. BARREIRO - DISTRICT 5

COMMISSIONER REBECA SOSA - DISTRICT 6

COMMISSIONER CARLOS A. GIMENEZ - DISTRICT 7

COMMISSIONER KATY SORENSON - DISTRICT 8

COMMISSIONER DENNIS C. MOSS - DISTRICT 9

COMMISSIONER SENATOR JAVIER SOUTO - DISTRICT 10

COMMISSIONER JOE A. MARTINEZ - DISTRICT 11

COMMISSIONER JOSE "PEPE" DIAZ - DISTRICT 12

COMMISSIONER NATACHA SEIJAS - DISTRICT 13

COUNTY MANAGER - GEORGE BURGESS

Office of Community and Economic Development

140 W. Flagler

Miami, FL 33128

## MLK Economic Development Corporation

AD HOC COMMITTEE

STREETSCAPE SUBCOMMITTEE

FACADES SUBCOMMITTEE

6114 NW 7th Avenue

Miami, FL

## Urban Resource Group

A Division of Kimley-Horn and Associates, Inc.

Urban Design and Landscape Architecture

1691 Michigan Avenue, Suite 400

Miami Beach, FL 33139

(305)673-2025

JUNE, 2005



## T A B L E O F C O N T E N T S

ACKNOWLEDGEMENTS .....	3
TABLE OF CONTENTS .....	4
VISION .....	7
INTRODUCTION .....	8
PROJECT BOUNDARY MAP .....	9
<b>I. STREETSCAPE BEAUTIFICATION MASTER PLAN.....</b>	<b>11</b>
INTRODUCTION - ELEMENTS OF DESIGN .....	12
CROSS-SECTIONS.....	14
NW 12th Ave. to NW 7th Ave - Existing.....	14
NW 12th Ave. to NW 7th Ave - Proposed.....	15
NW 7th Ave. to I-95- Existing.....	16
NW 7th Ave. to I-95- Proposed .....	17
ILLUSTRATIVE MASTER PLAN.....	18
TYPICAL CROSSWALK AND INTERSECTION LAYOUT .....	26
Primary Intersection .....	26
Secondary Intersection .....	27
Neighborhood Intersection.....	28
PUBLIC/PRIVATE RELATIONSHIP.....	29
LINEAR PARK DESIGN ALTERNATIVES.....	30
Alternative 1.....	30
Alternative 2.....	31
Alternative 3.....	32
PEDESTRIAN RAMP ALTERNATIVES .....	33
ILLUSTRATIVE IMAGES .....	34
Scenario 1 .....	34
Scenario 2 .....	36
TECHNICAL LAYOUT.....	38
STREETSCAPE ELEMENTS .....	47
Landscape Palette .....	48
Street Furniture Palette.....	55
Hardscape Palette.....	57
Opinion of Probable Construction Cost.....	59

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

<b>II. FACADE STANDARDS</b> .....	<b>61</b>
<b>INTRODUCTION - PRIVATE SECTOR</b> .....	<b>62</b>
<b>CONCEPT SKETCHES</b> .....	<b>63</b>
<b>POTENTIAL FACADE IMPROVEMENT SCENARIOS</b> .....	<b>66</b>
Scenario 1 .....	66
Scenario 2.....	68
Scenario 3.....	70
Potential Gateway Element .....	72
<b>GENERAL</b> .....	<b>74</b>
<b>FRONTAGE</b> .....	<b>75</b>
<b>TRANSITION ZONE</b> .....	<b>76</b>
<b>DOORS AND WINDOWS</b> .....	<b>77</b>
<b>ROOF</b> .....	<b>78</b>
<b>FACADE ARTICULATION</b> .....	<b>79</b>
<b>STOREFRONT</b> .....	<b>80</b>
<b>MATERIALS</b> .....	<b>82</b>
<b>IDENTIFICATION MARKERS</b> .....	<b>83</b>
<b>COLORS</b> .....	<b>84</b>
<b>SIGNAGE</b> .....	<b>90</b>
<b>AWNINGS</b> .....	<b>92</b>
<b>ARCADE</b> .....	<b>94</b>
<b>LIGHTING</b> .....	<b>96</b>
<b>STOREFRONT SECURITY</b> .....	<b>95</b>
<b>PARKING</b> .....	<b>97</b>
<b>GARBAGE DISPOSAL</b> .....	<b>98</b>
<b>MECHANICAL AND ELECTRICAL EQUIPMENT</b> .....	<b>99</b>
<b>APPENDIX</b> .....	<b>101</b>
<b>INVENTORY/ANALYSIS</b> .....	<b>102</b>
<b>HISTORICAL DESIGNATION REPORT</b> .....	<b>110</b>

## V I S I O N   F O R   M L K   B O U L E V A R D

The vision for Dr. Martin Luther King Boulevard...

- A place that provides a mix of learning, living, working, shopping and entertainment in a Tropical Afrocentric atmosphere.
- A place that is active, lively, and provides a distinctive urban lifestyle that reinforces Miami's multi-cultural and multi-generational characteristics.
- A place which combines a variety of building heights, shapes, with land uses geared to pedestrians; encourages window shopping, outdoor cafes, restaurants, and human interaction along wide sidewalks, plazas, open spaces, balconies and arcades.
- A place that provides a connection to Model City/Liberty City from Downtown Miami (via I-95 and US 441), and from the Upper Eastside/Biscayne Boulevard to the City of Hialeah (via Dr. MLK King Boulevard), provides adequate parking, discourages the use of the automobile at ground floor; and provides a place that has attractive colors, materials, lighting, and landscaping; that is safe to the public welfare.
- A place that provides a variety of affordable housing for all income and age groups, especially the middle class, provides residents, visitors and tourists with a variety of 24 hour services, including among others: restaurants, entertainment, cultural, and specialty retail.
- A neighborhood corridor that reinforces the messages and teachings of Dr. Martin Luther King, Jr., and the cultures of all people of the African Diaspora, including its music, cuisine, art, and traditions.



## I N T R O D U C T I O N

### The **Dr. Martin Luther King, Jr. Boulevard Streetscape Beautification Master Plan and Facade Standards**

serves to advance the overall vision established by the local community to redevelop the area both physically and economically. This document focuses on three primary components which constitute an important part of the overall community revitalization efforts. These components include:

- Creation of a **beautification master plan** for the MLK corridor itself which will serve as a blue print for future roadway improvements along the length of the corridor. The focus of these standards is to fast track the design and implementation of the streetscape improvements along the corridor while maintaining a meaningful and consistent theme throughout.

The Beautification Master Plan includes: The illustrative Master Plan, Existing and Proposed Cross-Sections, Concept Sketches of the Corridor, Typical Crosswalk and Intersection Layouts, Public/ Private Relationship Layouts, linear park design alternatives, streetscape element palette, technical layout, and opinion of probable construction cost.

- Establishment of specific design **standards for façade improvements** which will aid local property owners in enhancing the physical characteristics of their properties. These standards are not intended to restrict design, but rather to establish an envelope in which the character of the neighborhood will be emphasized through quality architectural design.

The **facade standards** portion of this report will include specific design standards for architectural façade improvements such as roofs, walls, color, lighting, signage, windows, doors, trim, security, transparency, building frontage, building materials, building scale, mechanical equipment screening, on-site parking screening, and architectural articulation.

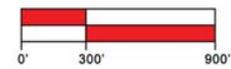
- **Historical Designation Reports** for potential historic sites that played and continue to contribute to the history and character of the neighborhood. These reports will be submitted to the State for the consideration of establishing several historic properties along the corridor as official state designated historic sites.

This section of the document will include the various reports for the potential historical designation sites which are: The Segregation Wall, The Carver Theater, The Labor Union Hall, Yeager Clinic, Post Office Building, Pallbearer's Building and the Continental Cleaner's Building.

These efforts combined will serve to better the community and advance the effort of revitalization. The Beautification Master Plan for the public improvements will work in tandem with the proposed façade standards to establish a cohesive and unified theme and character for the corridor which symbolically **expresses the strides and accomplishments of Dr. King as an individual as well as the Civil Rights movement in Miami.**

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

## PROJECT BOUNDARY MAP



STREETSCAPE  
BEAUTIFICATION  
MASTER PLAN

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

## INTRODUCTION

### Streetscape Beautification Master Plan Elements of Design

The streetscape beautification section of this plan will serve as a guide for future improvements to the public right-of-way. Through the public process, the theme and vision for the corridor was established. The theme and vision focus on MLK as a man and leader, symbolically highlighting his life's accomplishment, but also what his persona represents: coming together and freedom. The vision is further characterized by African-based motifs, patterns and colors which relate to the cultural make-up of the neighborhood. It is the intent of this beautification master plan to establish a standard for design that implements the theme and vision throughout the corridor.



The beautification master plan will regulate various design elements throughout the streetscape, including:

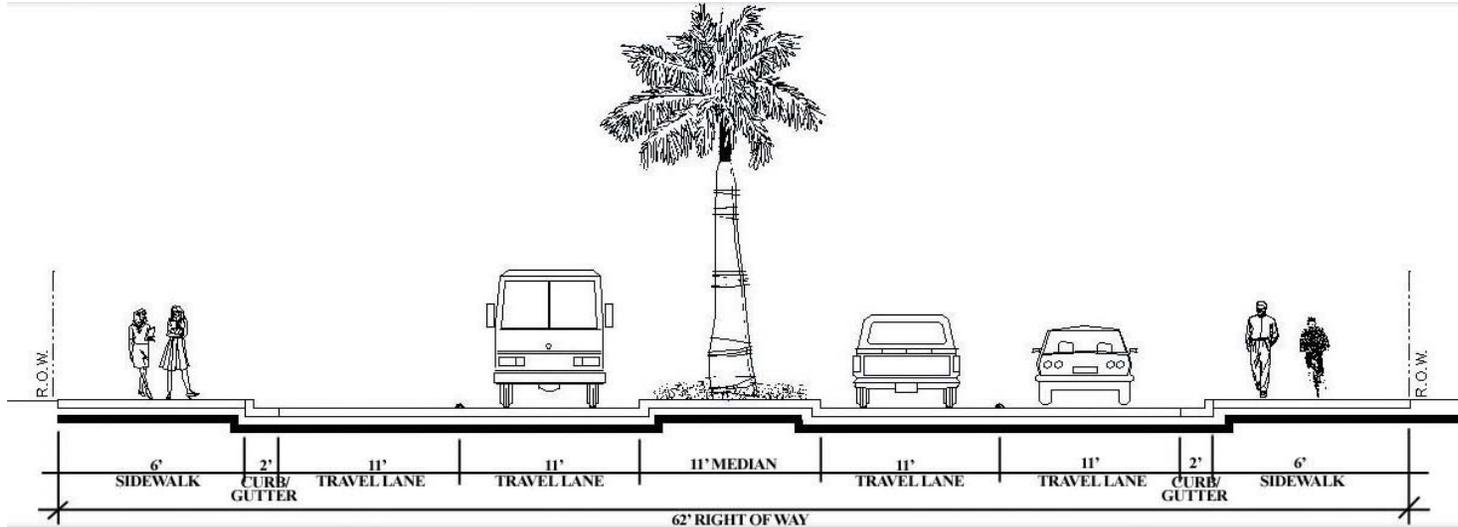
- Hardscape Materials (i.e. sidewalk, pavers, etc)
- Planting
- Lighting
- Linear Park Redesign
- Crosswalks



# DR. MARTIN LUTHER KING, JR. BOULEVARD

## EXISTING CROSS SECTION

MLK Boulevard from NW 12th Avenue to NW 7th Avenue

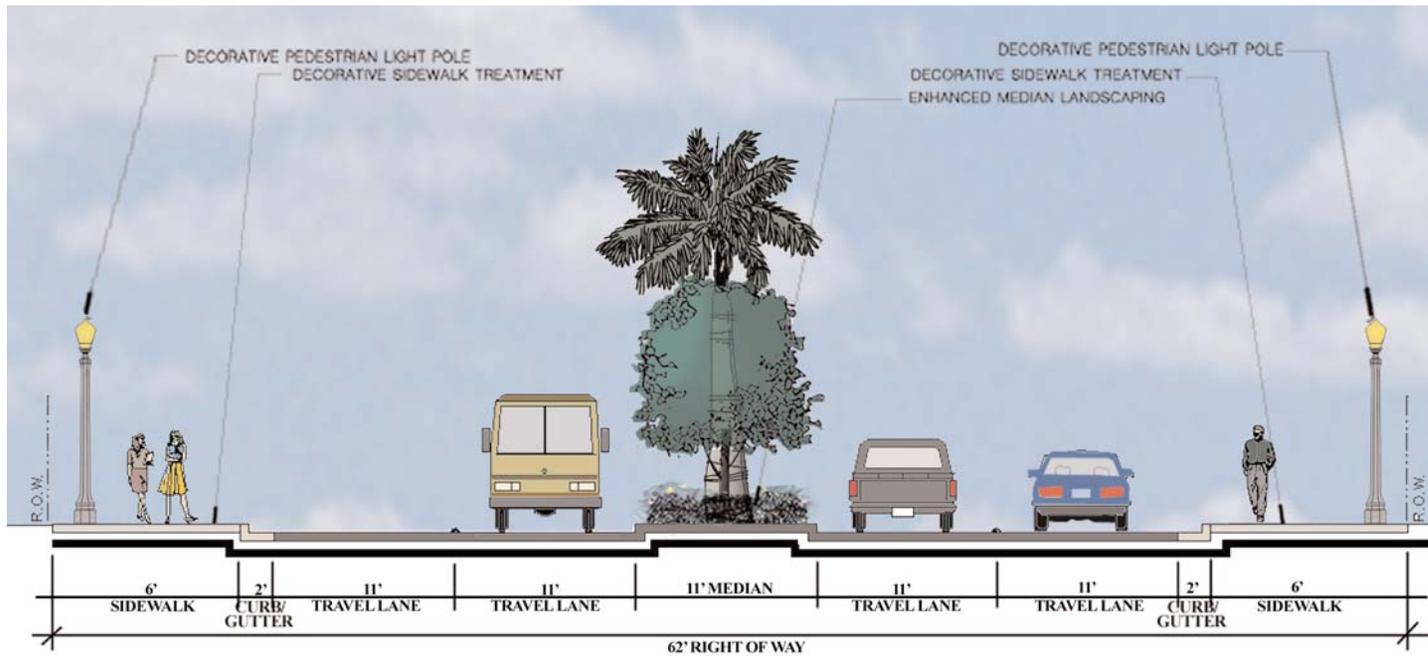


NOT TO SCALE

This cross section represents the typical condition along NW 12th Avenue to NW 7th Avenue. The medians are planted but with minimal vegetation while the sidewalks are, for the most part, narrow and void of landscaping except for areas along the police station and Tacolcy Center.

## PROPOSED CROSS SECTION

MLK Boulevard from NW 12th Avenue to NW 7th Avenue



NOT TO SCALE

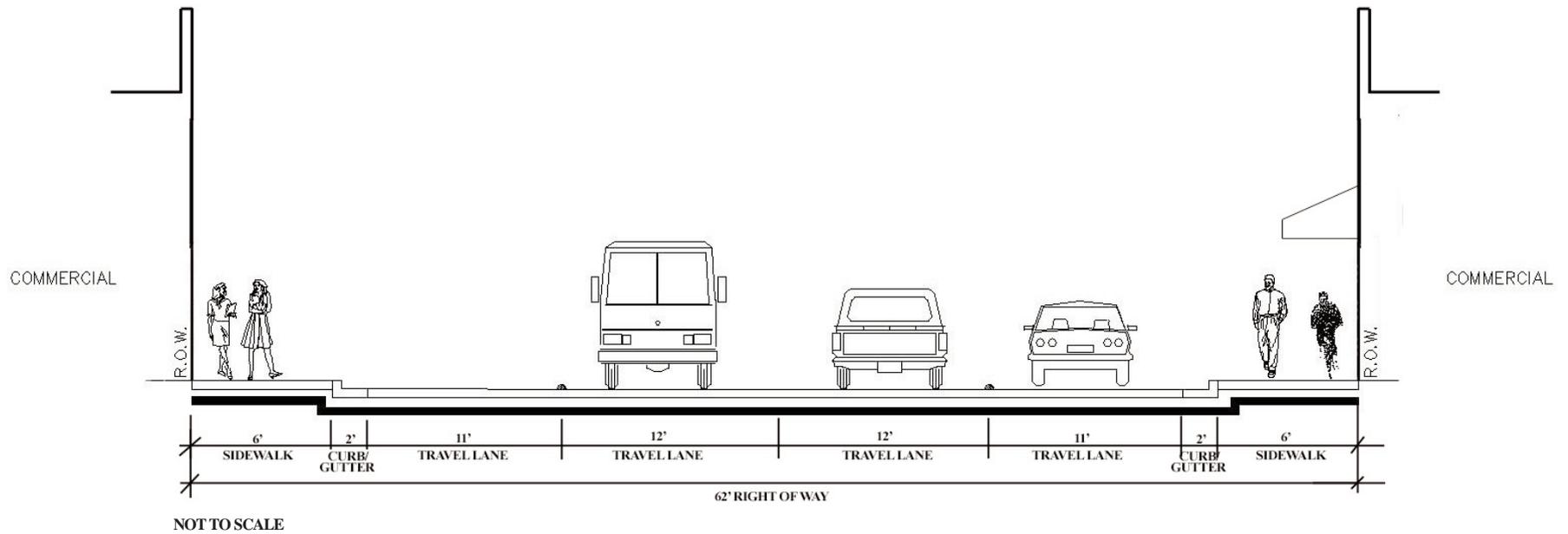
The proposed cross-section above will enhance the overall appearance of the streetscape by adding decorative pedestrian light poles, new concrete sidewalks, and enhancing the median and sidewalk landscaping where possible.



# DR. MARTIN LUTHER KING, JR. BOULEVARD

## EXISTING CROSS SECTION

MLK Boulevard from NW 7th Avenue to I-95



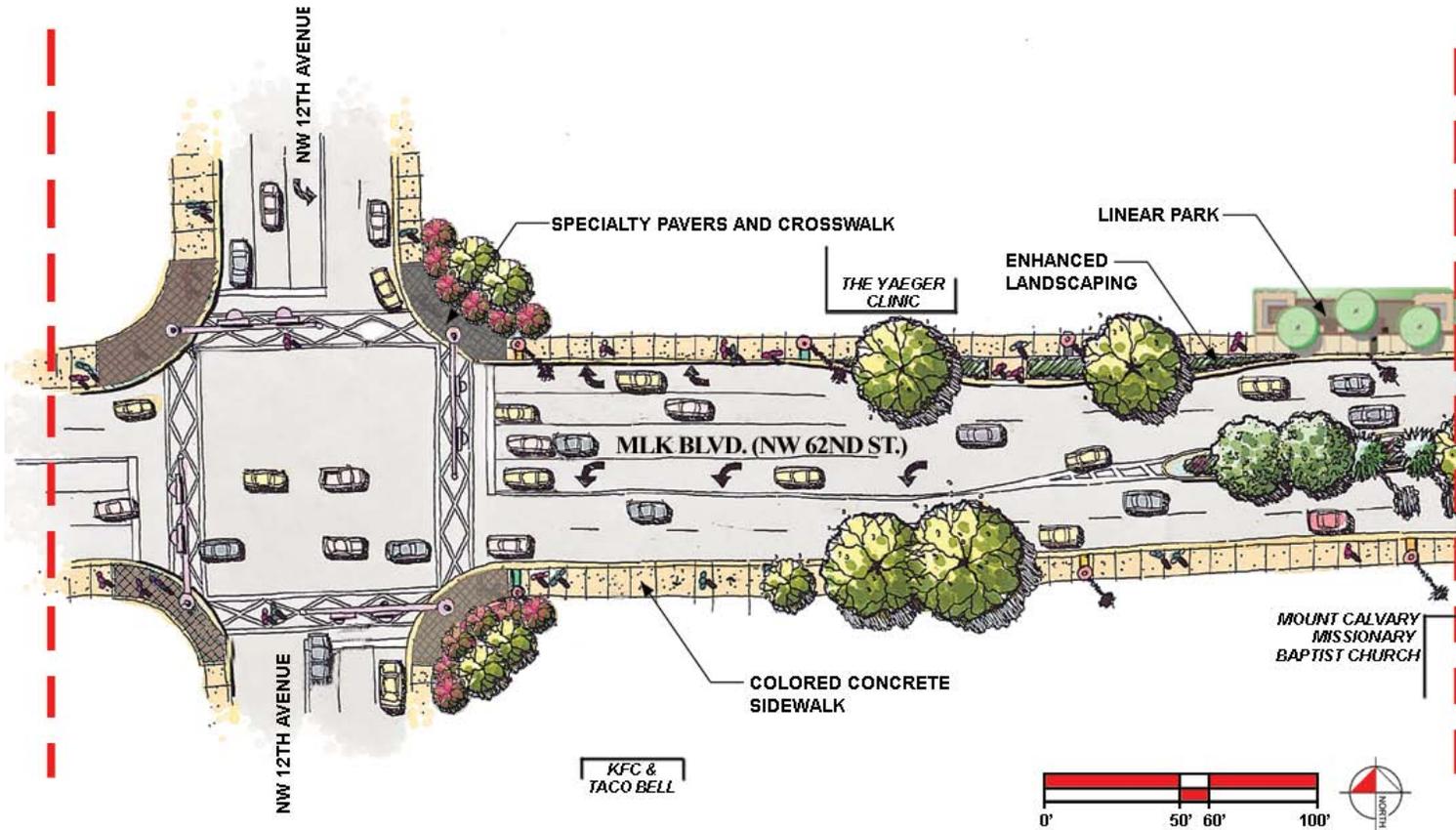
The typical cross-section for this portion of the corridor is characterized by commercial buildings against the back of sidewalk with no landscaping and no center median.





# DR. MARTIN LUTHER KING, JR. BOULEVARD

## ILLUSTRATIVE MASTER PLAN



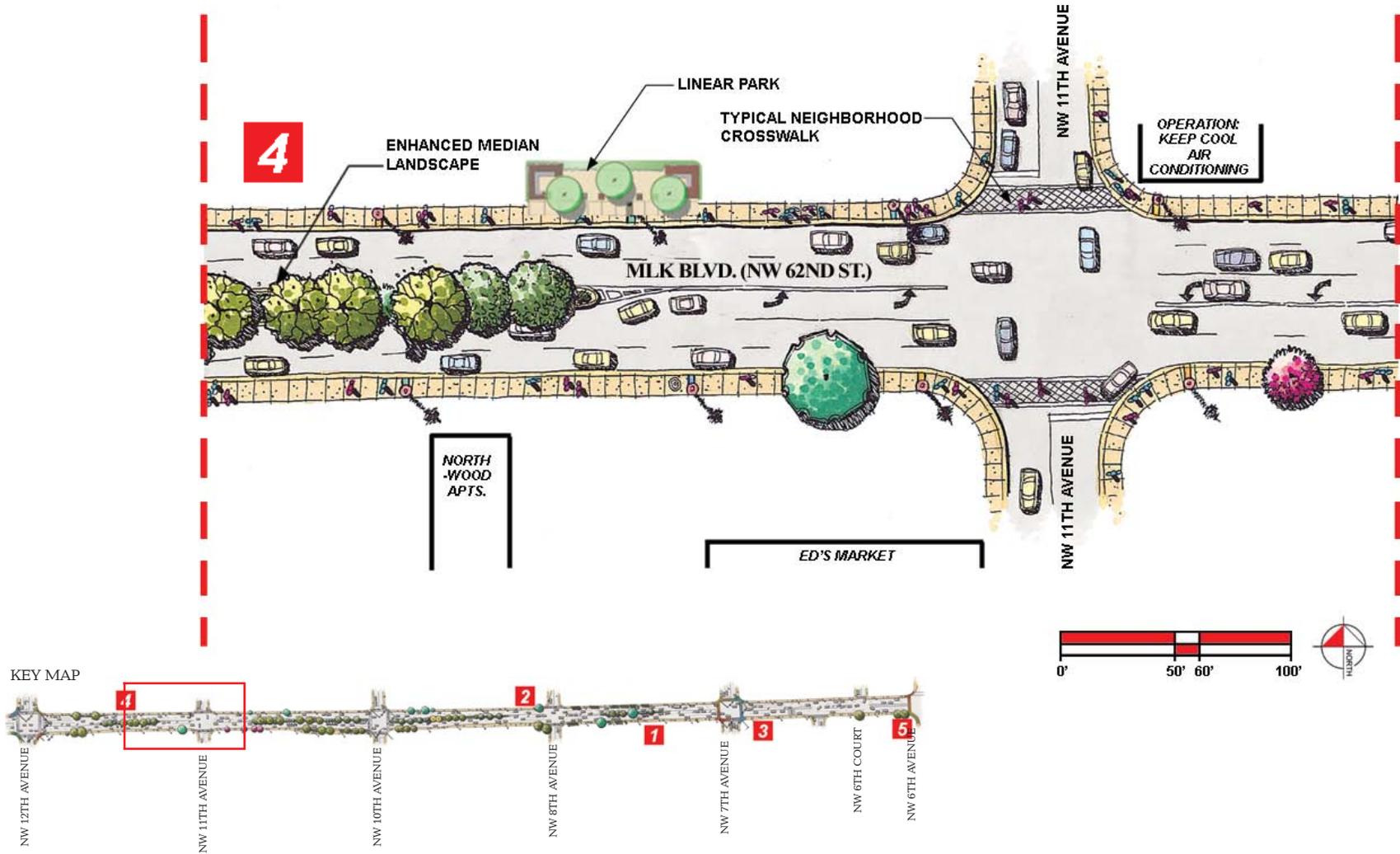
KEY MAP



\* Refer to technical layout for specific material information and locations of streetscape elements.

**5** Refers to location of historically significant buildings (typ.)

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

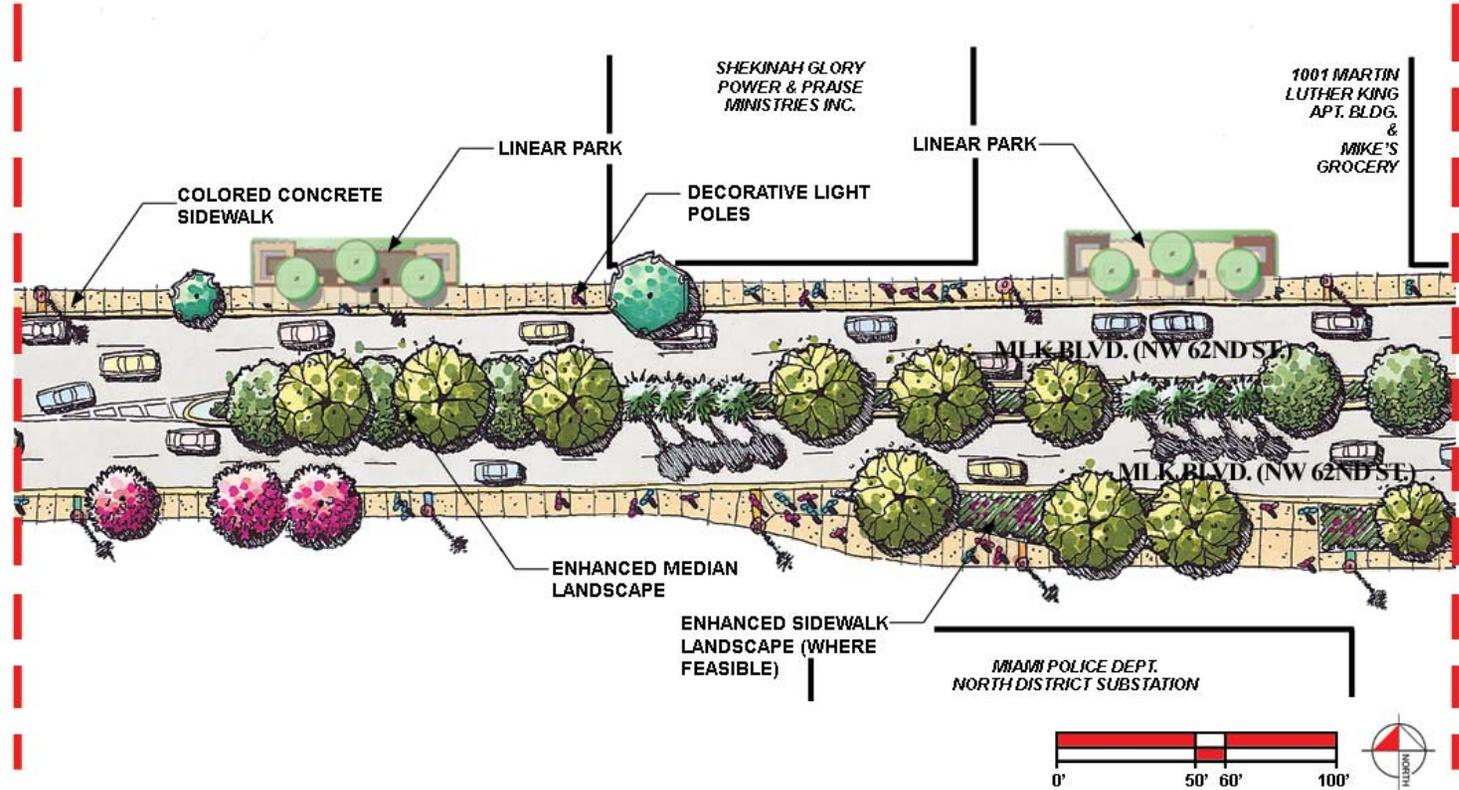


\* Refer to technical layout for specific material information and locations of streetscape elements.

**5** Refers to location of historically significant buildings (typ.)



# DR. MARTIN LUTHER KING, JR. BOULEVARD



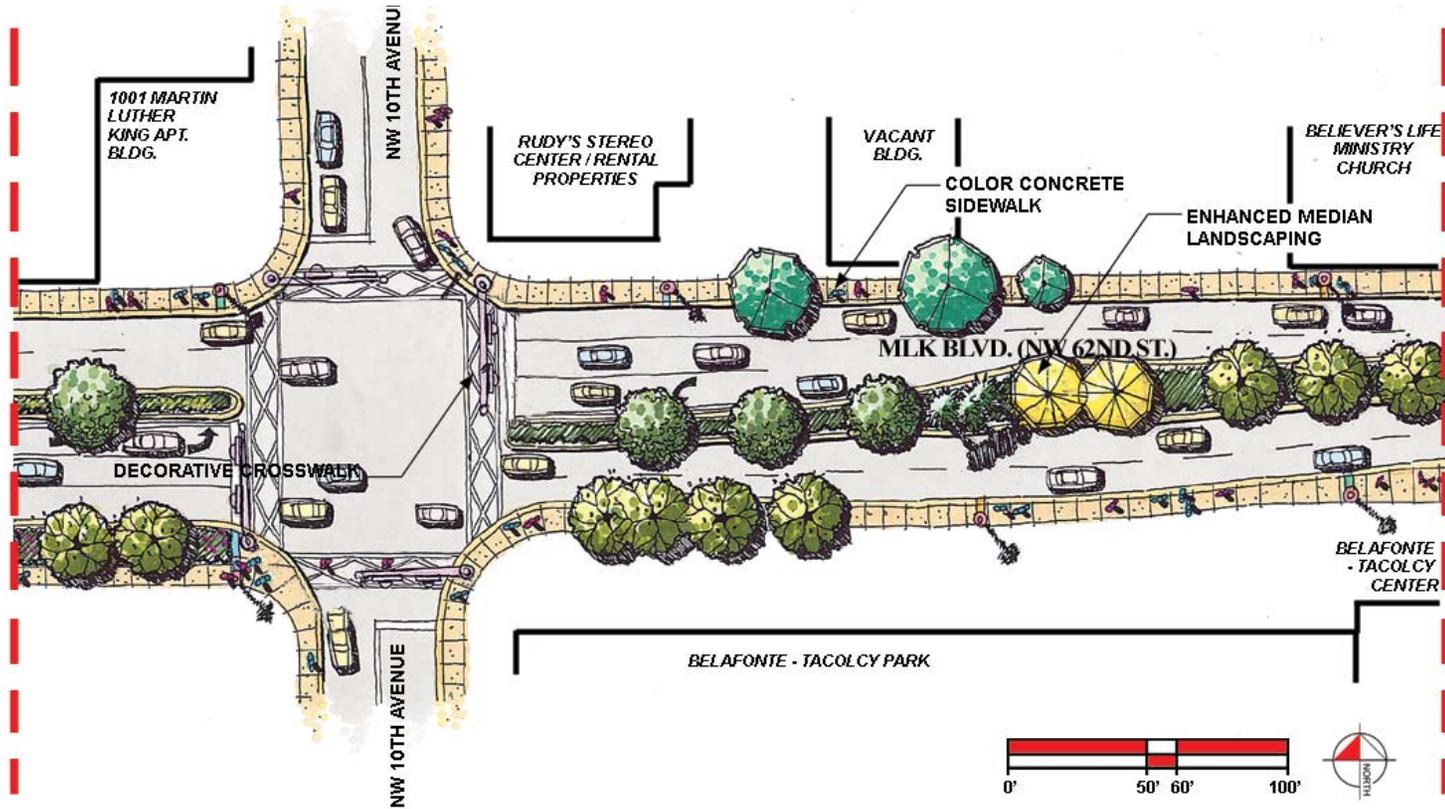
KEY MAP



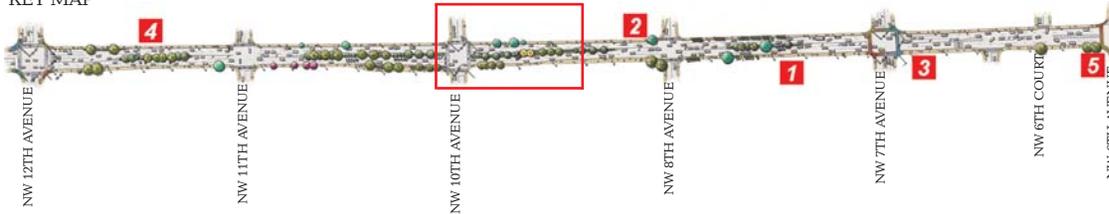
\* Refer to technical layout for specific material information and locations of streetscape elements.

5 Refers to location of historically significant buildings (typ.)

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS



KEY MAP

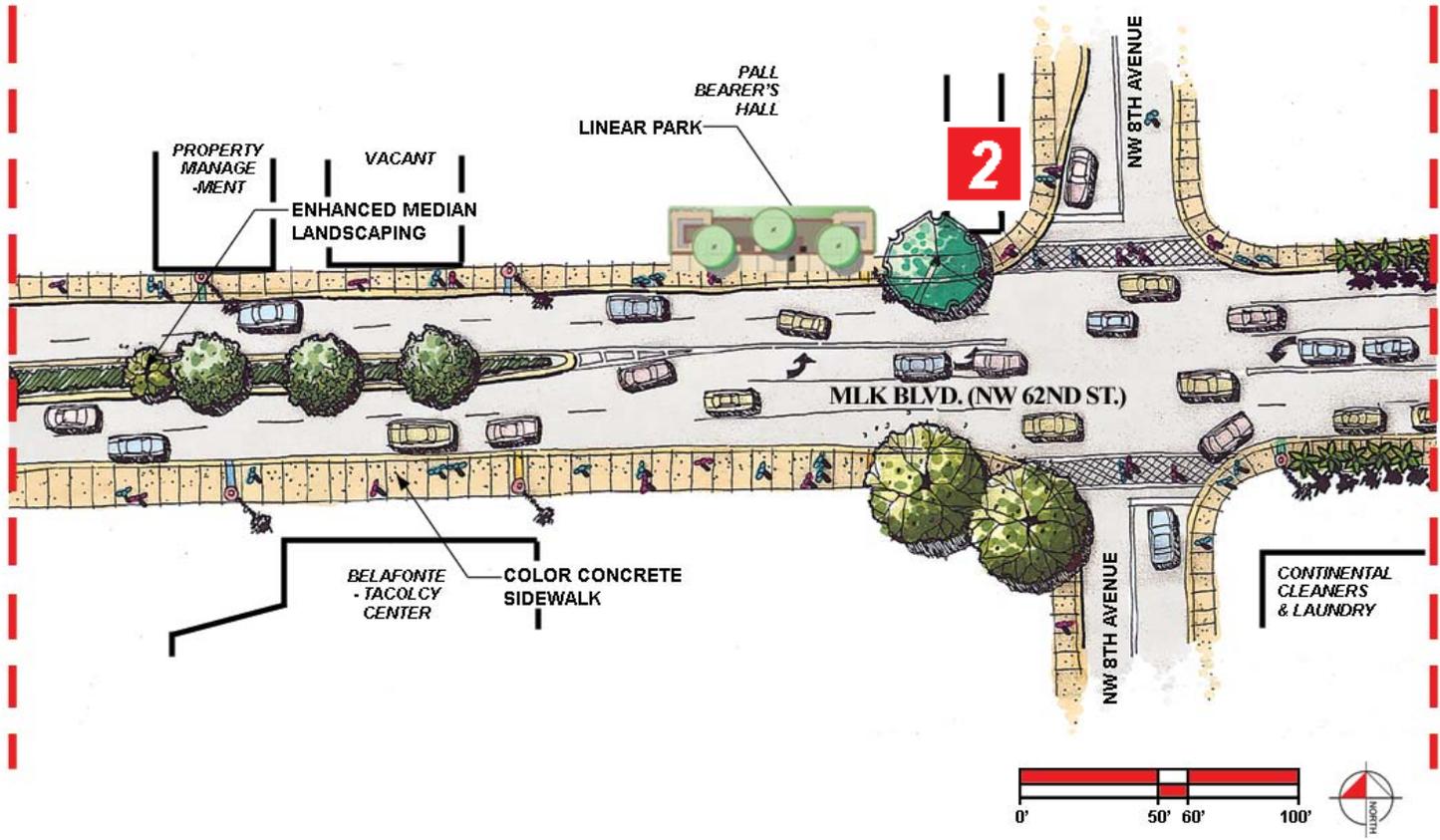


\* Refer to technical layout for specific material information and locations of streetscape elements.

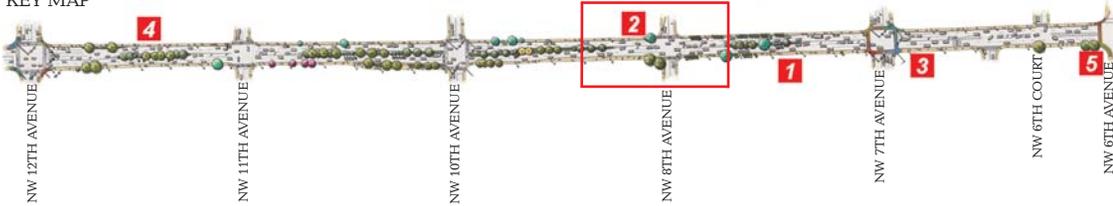
**5** Refers to location of historically significant buildings (typ.) 21



# DR. MARTIN LUTHER KING, JR. BOULEVARD



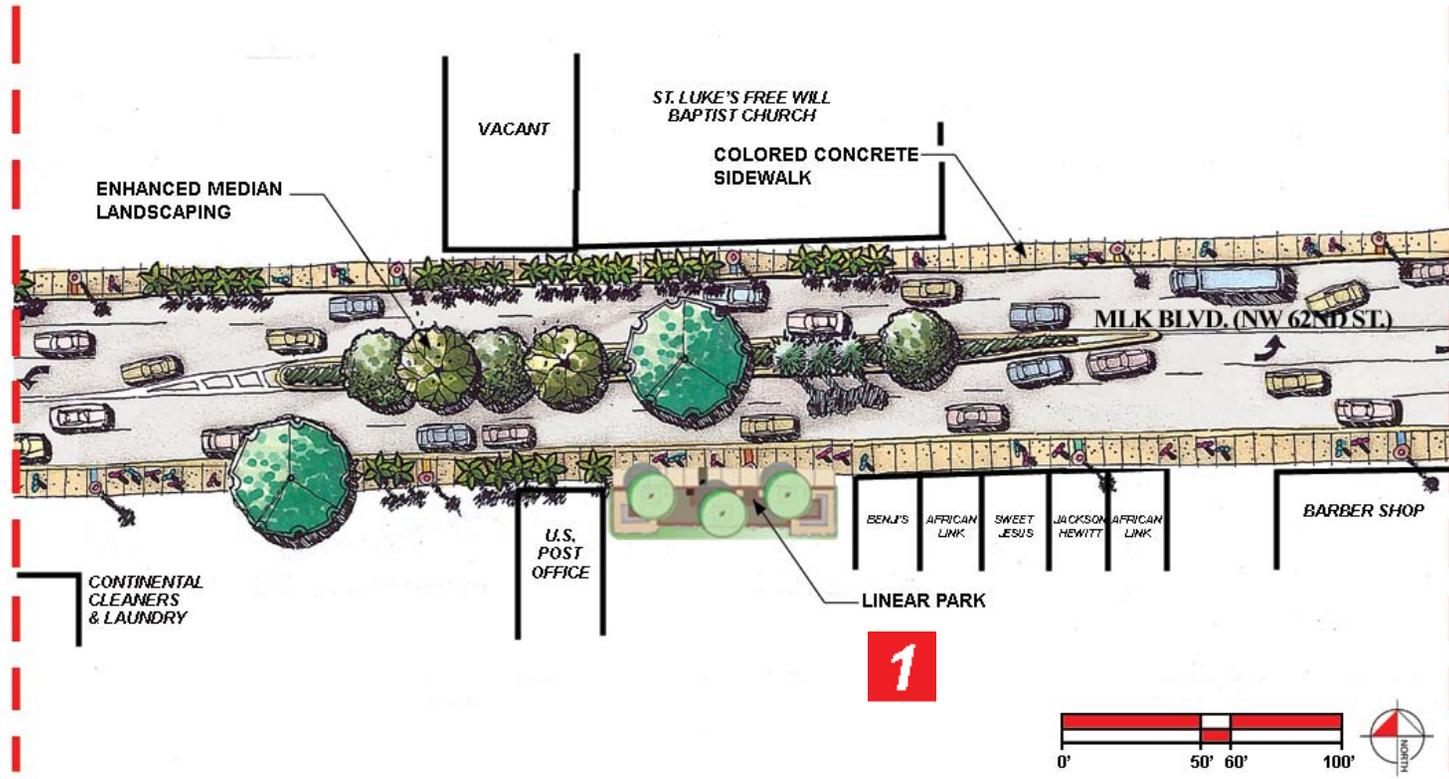
KEY MAP



\* Refer to technical layout for specific material information and locations of streetscape elements.

5 Refers to location of historically significant buildings (typ.)

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS



KEY MAP

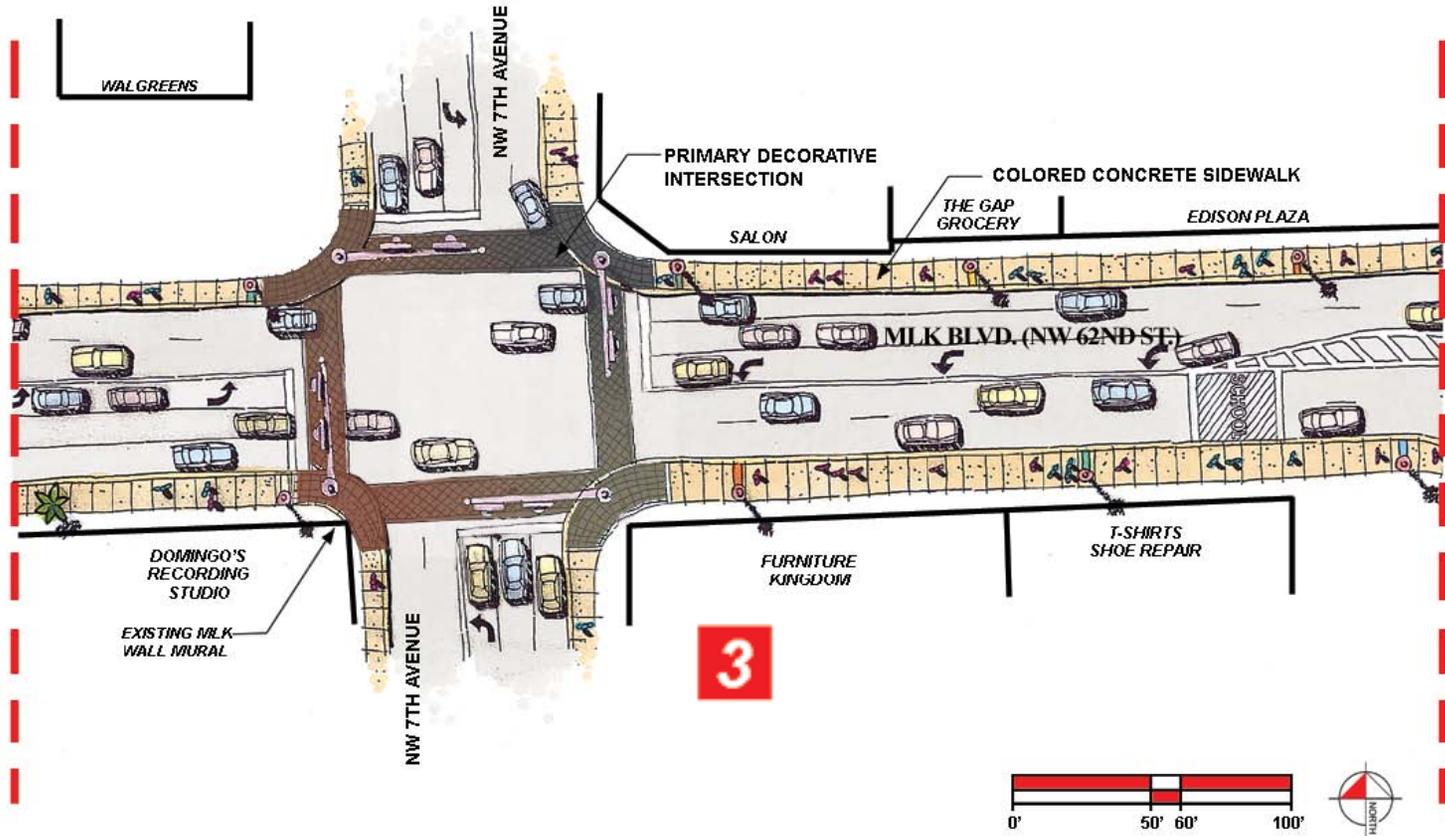


\* Refer to technical layout for specific material information and locations of streetscape elements.

5 Refers to location of historically significant buildings (typ.)



# DR. MARTIN LUTHER KING, JR. BOULEVARD



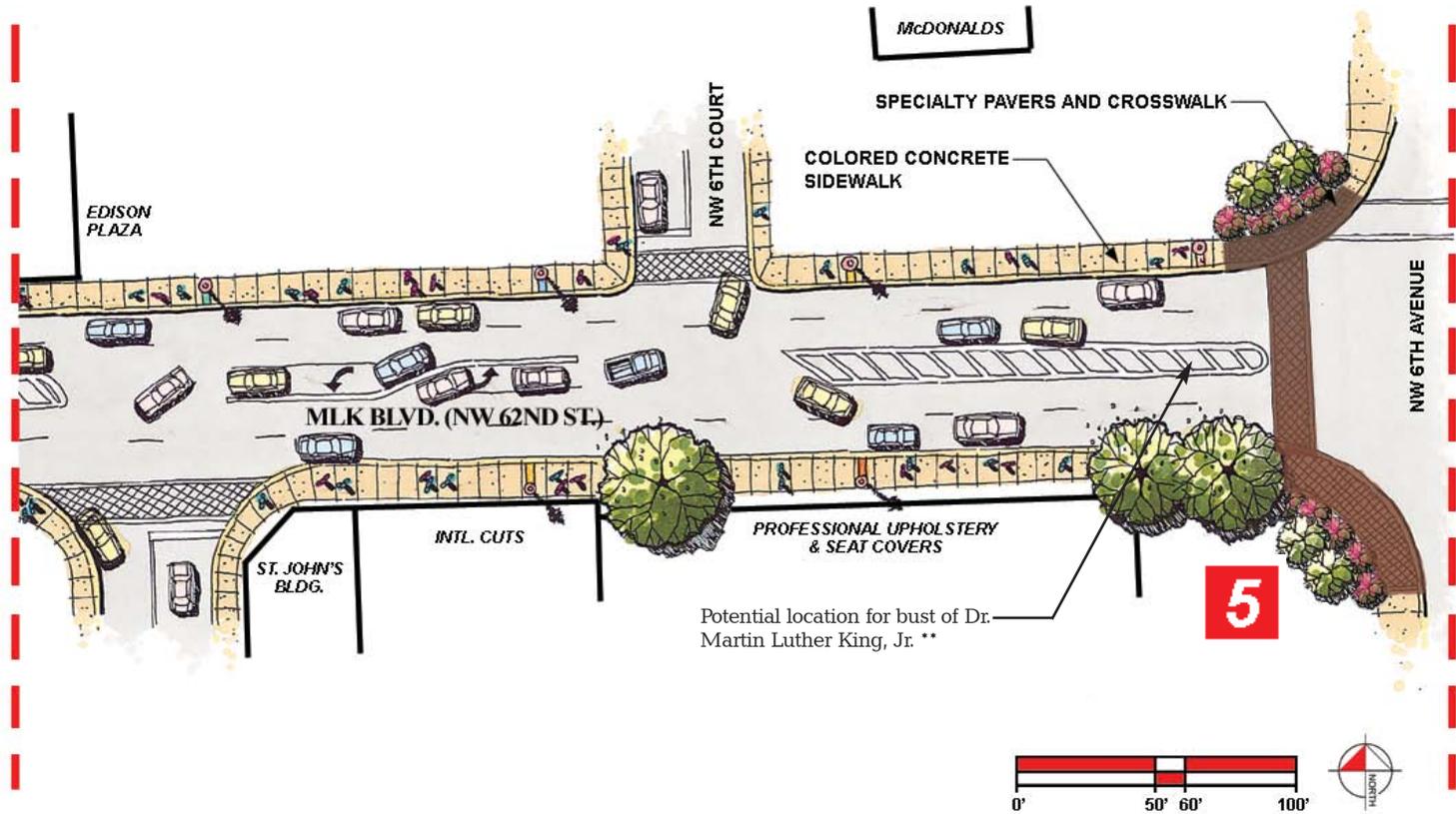
KEY MAP



\* Refer to technical layout for specific material information and locations of streetscape elements.

**5** Refers to location of historically significant buildings (typ.)

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS



KEY MAP



\* Refer to technical layout for specific material information and locations of streetscape elements.

\*\* To be coordinated with Miami Dade County Roadway Improvements Project.

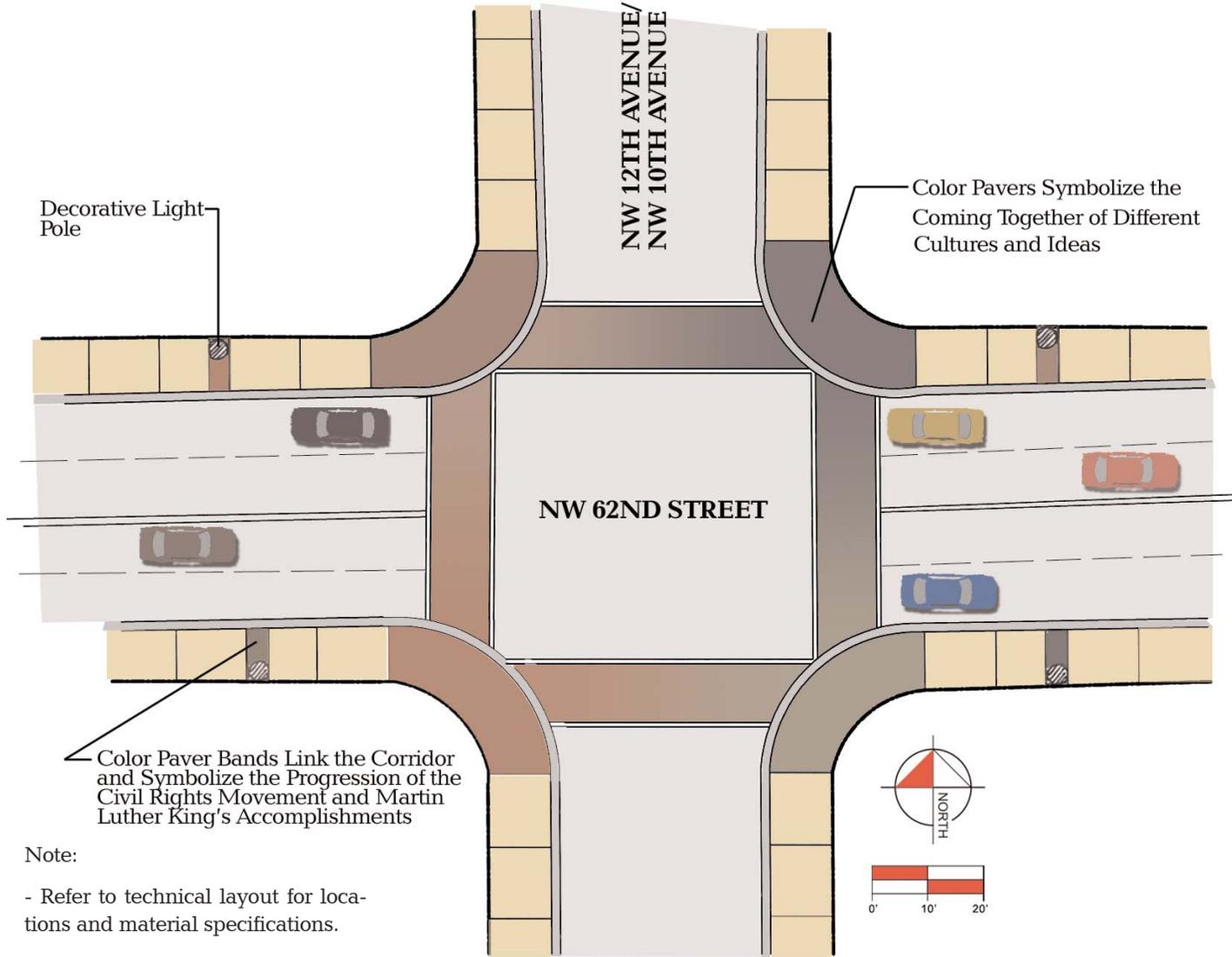
**5** Refers to location of historically significant buildings (typ.) 25



# DR. MARTIN LUTHER KING, JR. BOULEVARD

## TYPICAL CROSSWALK AND INTERSECTION LAYOUT

### Primary Intersection



Dr. Martin Luther King, Jr. was a man who worked relentlessly to bring cultures together during the Civil Rights Movement. His efforts are seen as a driving force in gaining equal rights for African Americans. Because of Dr. King, the efforts toward equality continue and many other cultures have been impacted.

The design of the intersections along Dr. Martin Luther King Jr. Boulevard reflects his efforts and the coming together of different cultures and ideas. At the primary intersections, at NW 6th Avenue and MLK Boulevard and at NW 7th Avenue and MLK Boulevard, different colored pavers serve to represent different cultures. As the colors move across the crosswalk, they bleed into each other, blurring the line of differences.

Along the entire corridor, where lighting exists, an alternating colored paver band occurs both to unify the design of the corridor, and symbolically show the progression of the civil rights movement. At each band will be a physical element with descriptions of Dr. King's accomplishments, his speeches, his actions, etc.

Note:  
- Refer to technical layout for locations and material specifications.

NW 6th Avenue and MLK Boulevard  
NW 7th Avenue and MLK Boulevard

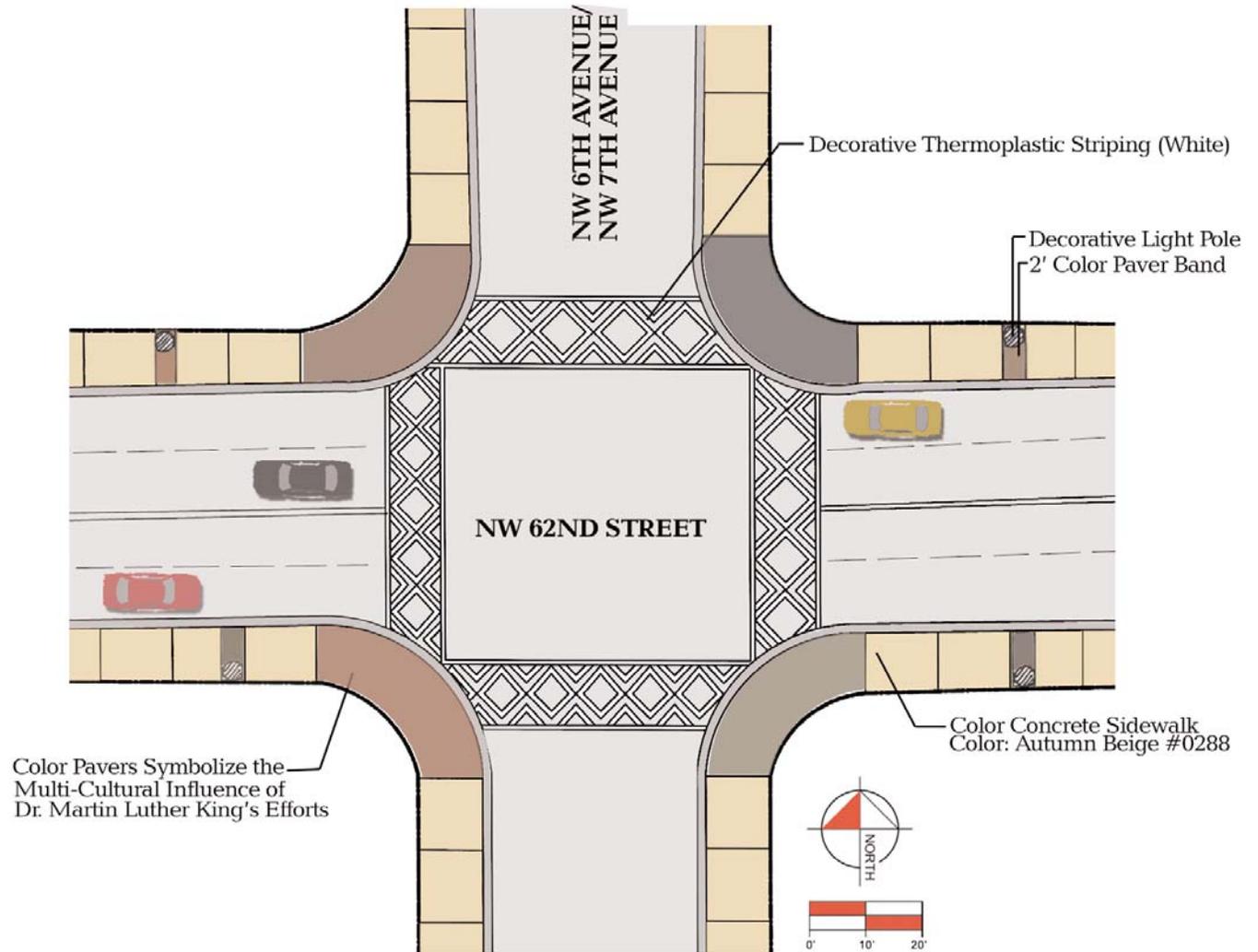
## Secondary Intersection(Typical)

At secondary intersections, the same colored pavers are used to symbolize different cultures and ideas, but the colors do not bleed together. This creates a hierarchy of streets along the corridor. The crosswalks in these secondary intersections use decorative striping.

The same paver bands continue throughout the corridor at the light fixtures.

Note:

- Refer to technical layout for locations and material specifications.
- Refer to Opinion of Probable Cost on Page 68 for Cost Estimate.

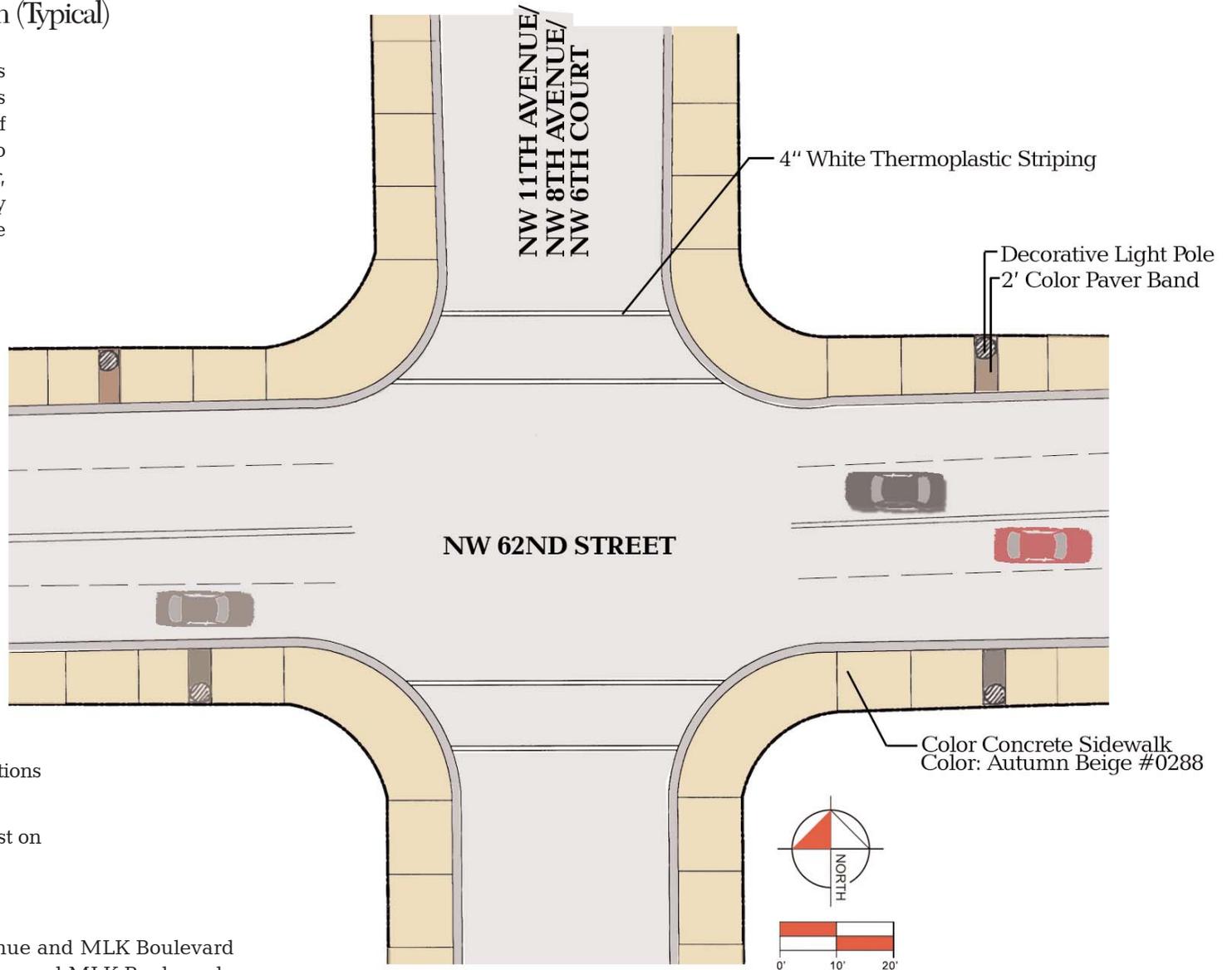


NW 12th Avenue and MLK Boulevard  
 NW 10th Avenue and MLK Boulevard



## Neighborhood Intersection (Typical)

Following the hierarchy, this intersection treatment represents the third tier in the sequence of the streetscape. The aim is to direct focus toward the other, more prominent intersections by simplifying the design of these smaller intersections.



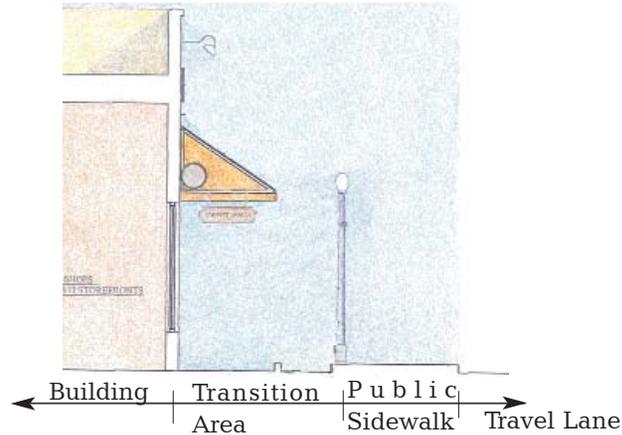
**Note:**

- Refer to technical layout for locations and material specifications.
- Refer to Opinion of Probable Cost on page 68 for cost estimate.

NW 11th Avenue and MLK Boulevard  
 NW 8th Avenue and MLK Boulevard  
 NW 6th Court and MLK Boulevard

## PUBLIC / PRIVATE RELATIONSHIP

In order to create a more pedestrian oriented street, the public/private relationship applies to the areas between the right of way and the building facade as is seen in the sketch below. The facade shall be mostly transparent - (see page 74 for storefront transparency standards). This transition area can be used for three things: 1) as an extension of the sidewalk, 2) as a seating area, and 3) as a landscape area. Refer to page 70 - Transition Zone for additional information.



### 1. Extension of the Sidewalk



The sidewalk in the image above is extended to provide a larger connection both to the building and to the street. In addition, it encourages more pedestrian activity in the area.

### 2. Seating Area



Above are two good examples of using the transition area as a space for outdoor seating. It draws more business while still allowing for unobstructed sidewalks.

### 3. Landscape Area

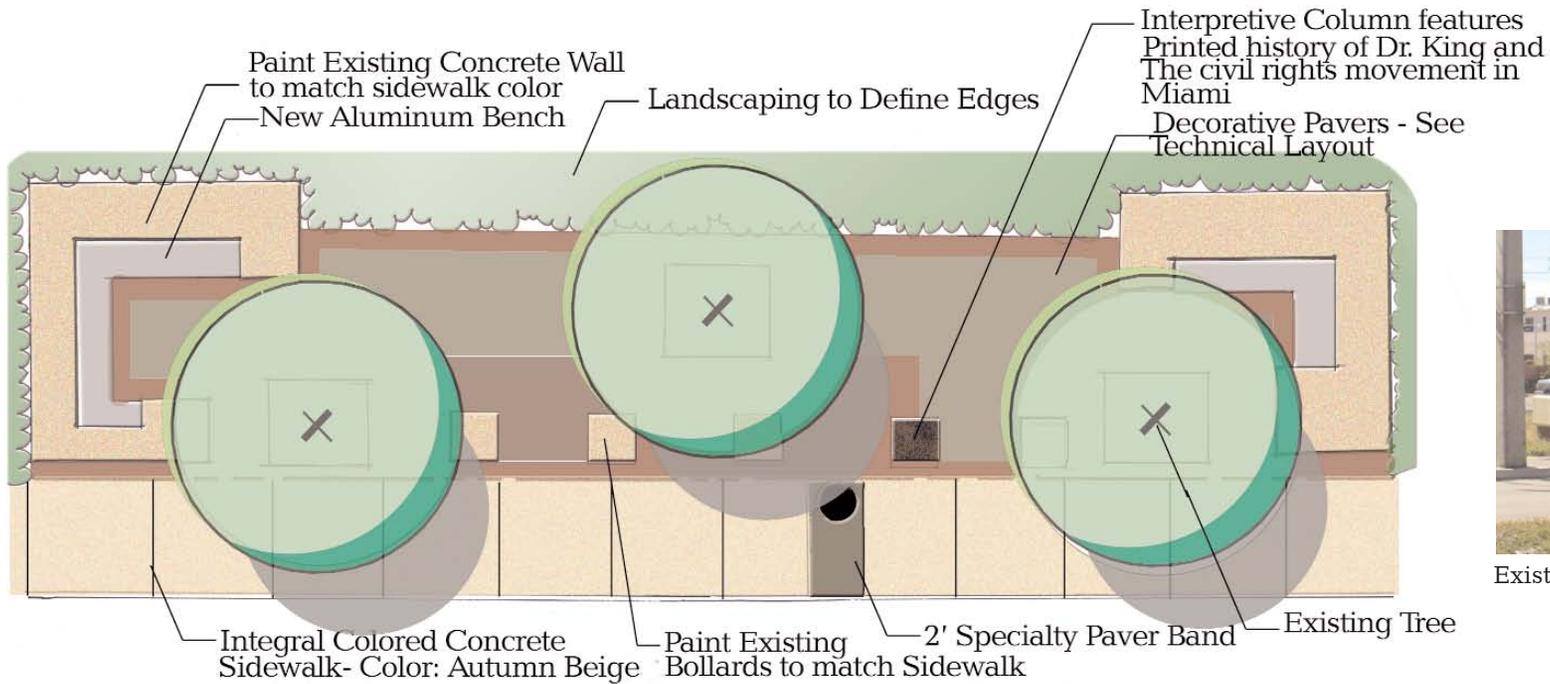


This neighborhood street in West Palm Beach is a good example of using a landscape area to delineate between the street and the sidewalk, and the sidewalk and the building face.

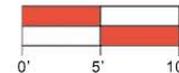


## L I N E A R P A R K D E S I G N A L T E R N A T I V E S

### ALTERNATIVE 1



Existing Linear Park.



Multi-colored paver design carries the theme of the many contributions that Dr. Martin Luther King made to cultures and races of the world. This theme is carried through into other components of the streetscape, such as decorative street crossings, which use the same colors. The 2' specialty paver band will carry the multi-colored/multi-cultural theme throughout the corridor. It will occur at each light fixture.

\*Refer to Opinion of Probable Cost on page 53 for cost estimate.

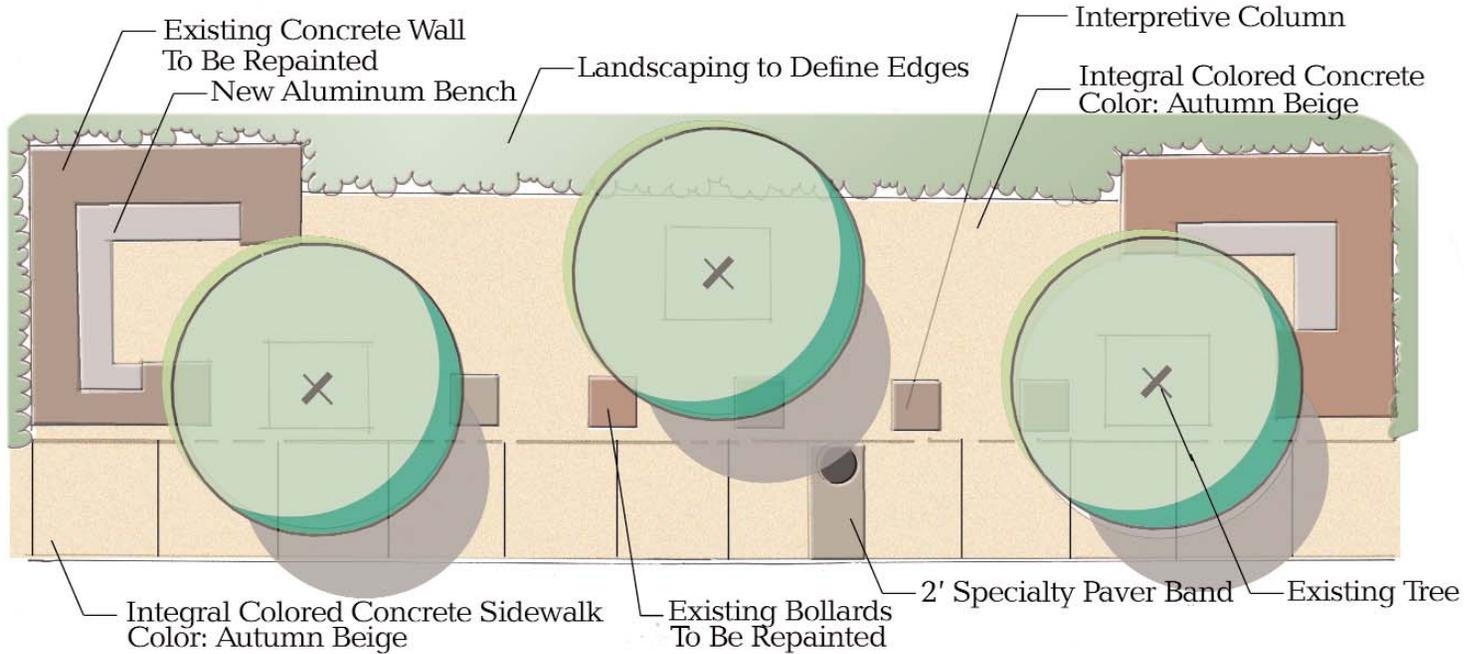
\*Refer to Technical Layout for linear park locations along MLK Boulevard.

\*Linear park sizes vary greatly between 15' and 50'.

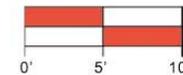
\*The potential memorials along the corridor may be located within the linear parks and other open space areas, such as medians.

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

## ALTERNATIVE 2



Interpretive Column



This is a lower cost alternative which allows the sidewalk material and color to enter the park. The multi-colored elements are limited to the existing walls and columns, which will be repainted. The multi-colored elements show the joining together of different cultures, which Dr. King helped to create.

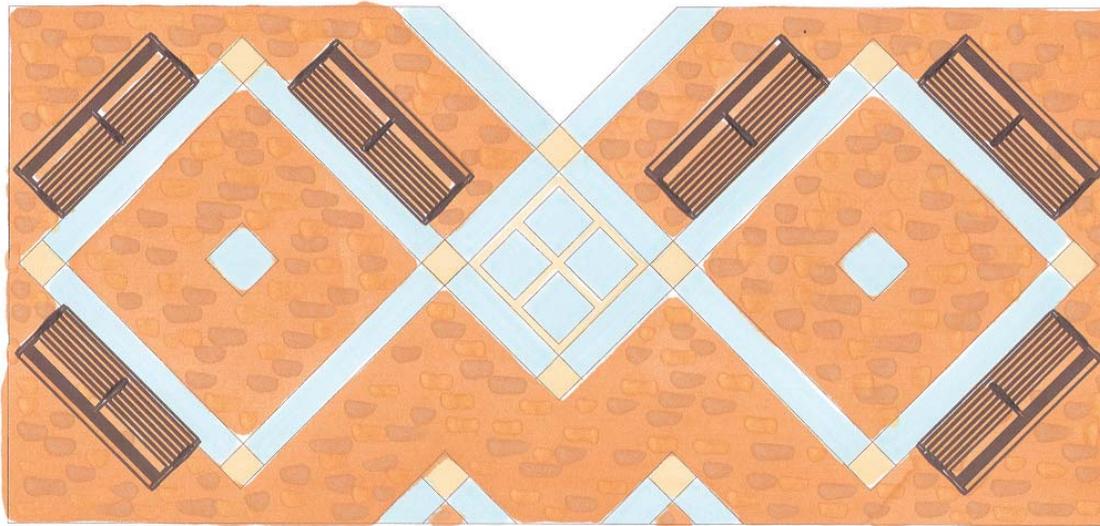
\*Refer to Opinion of Probable Cost on page 53 for cost estimate.

\*Refer to Technical Layout for Linear Park Locations

\*Linear park sizes vary greatly between 15' and 50'.



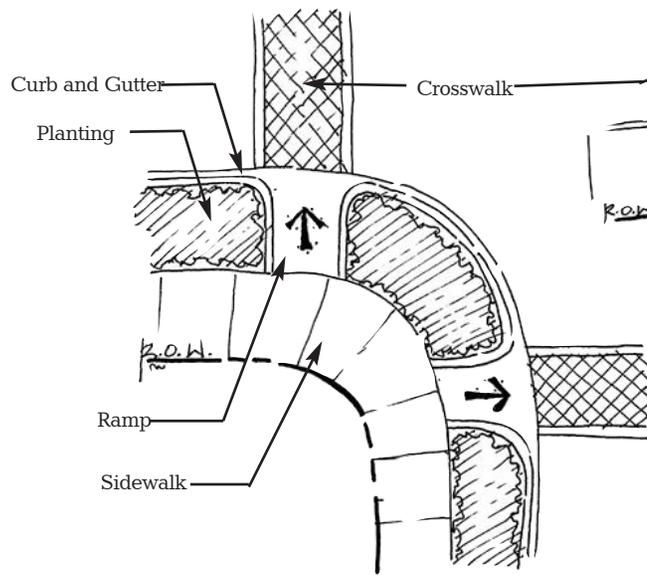
## ALTERNATIVE 3



This is an alternative solution that looks at removing the existing concrete walls and flooring and replacing these items with new pre-fabricated benches and new concrete flooring. The MLK interpretive elements would be incorporated into the various elements of the design.

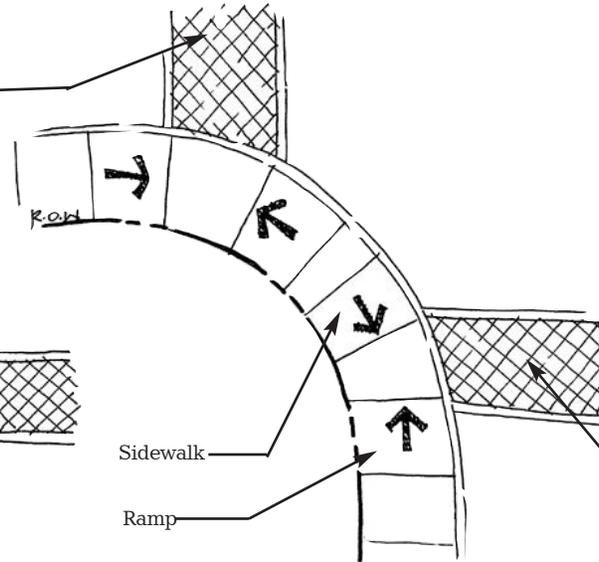
## P E D E S T R I A N R A M P A L T E R N A T I V E S

Preferred Option



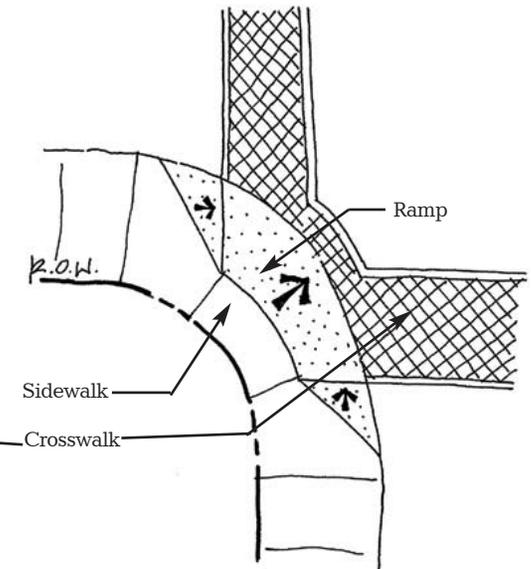
This pedestrian ramp detail moves the pedestrian crossing away from the intersection and reduces the width of the actual crossing. Refer to FDOT standards for ramp and crosswalk standards.

Preferred Limited Right-of-Way Option



This option can be used where there is limited right-of-way. This detail also moves pedestrian crossing activity away from the intersection. Refer to FDOT standards for ramp and crosswalk standards.

Secondary Option



This ramp option may be utilized where the other options are not feasible for a particular corner. Refer to FDOT standards for ramp and crosswalk standards.



# DR. MARTIN LUTHER KING, JR. BOULEVARD

## I L L U S T R A T I V E I M A G E S

Scenario 1 - Before Improvements



Outdated Signage

Exposed chain-link fence

Non-decorative light poles

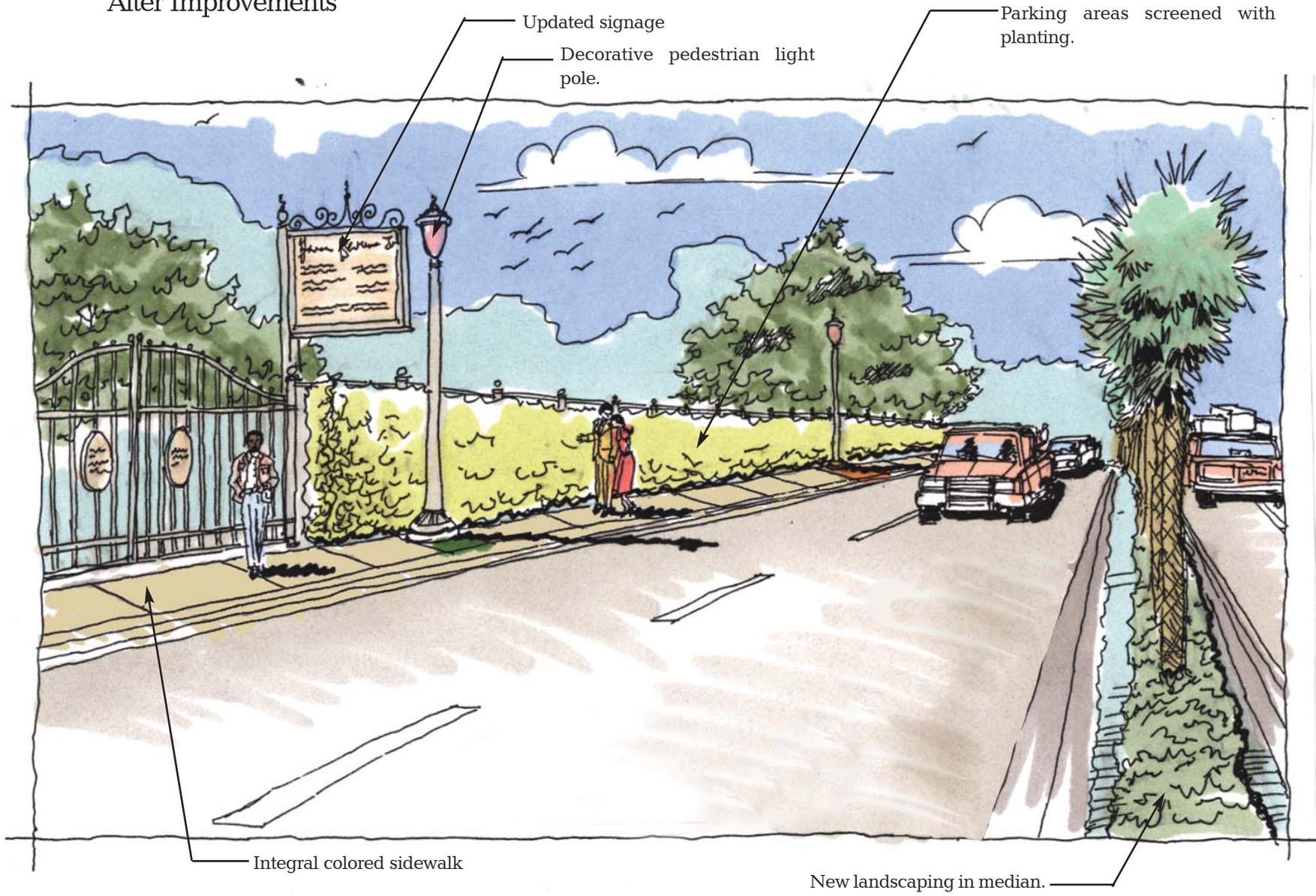
Views to parking

Grey Sidewalk

No landscaping in median.

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

After Improvements





## Scenario 2 - Before Improvements



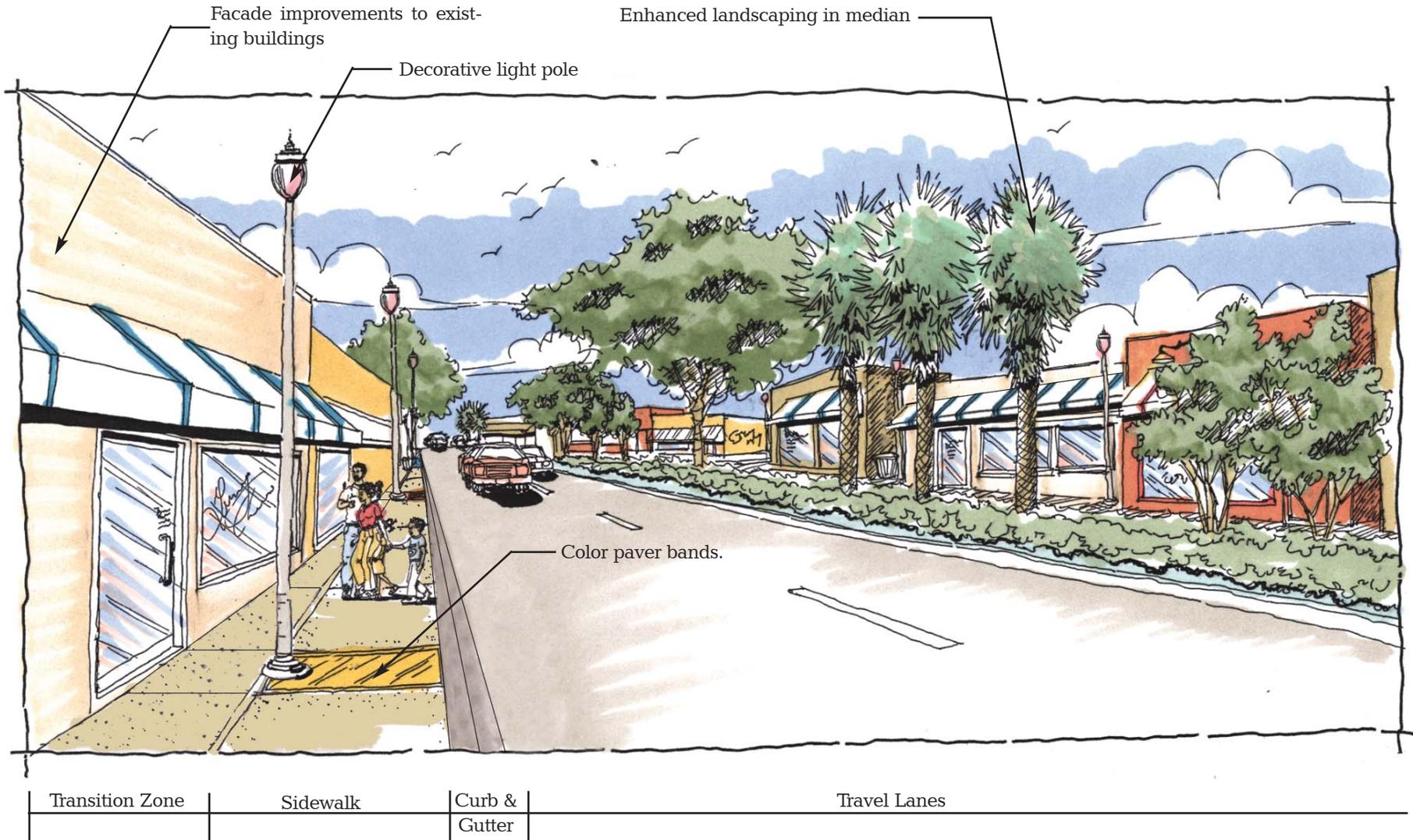
No Decorative Lighting

Blank facades

Lack of landscaping in median

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

## After Improvements





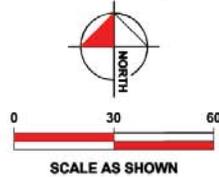
# DR. MARTIN LUTHER KING, JR. BOULEVARD

## TECHNICAL LAYOUT



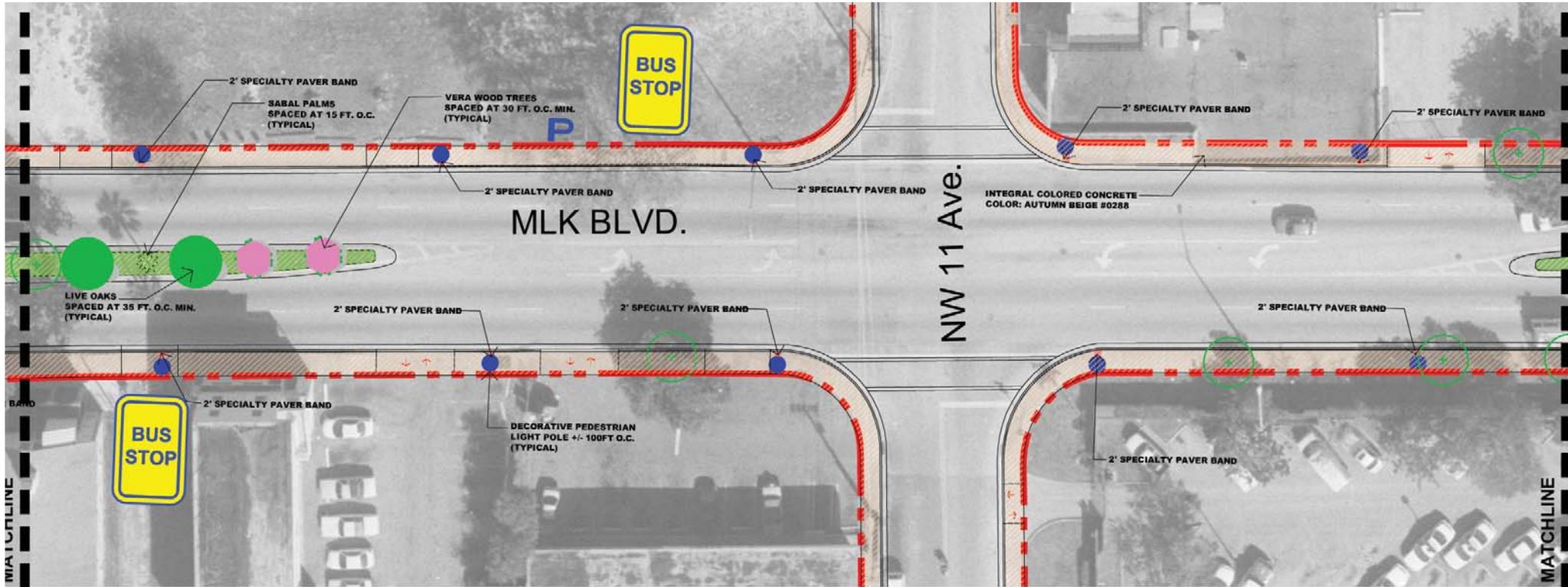
**NOTE:**  
REFER TO THE STREETSCAPE ELEMENTS LIST FOLLOWING THE TECHNICAL LAYOUT FOR SPECIFIC STREETSCAPE MATERIALS AND SPECIFICATIONS.

LEGEND:					
	RIGHT OF WAY		EXISTING SIGNALIZED INTERSECTION		PROPOSED CANOPY TREES
	EXISTING EDGE OF PAVEMENT		LINEAR PARK LOCATION		PROPOSED FLOWERING TREES
	SIDEWALK ZONE		EXISTING AND PROPOSED DRIVEWAYS		PROPOSED PALMS
	LANDSCAPE AREA		PROPOSED PEDESTRIAN LIGHT POLES		EXISTING CANOPY TREE
	DECORATIVE CORNER TREATMENT		PROPOSED GATEWAY FEATURE		EXISTING PALMS
	EXISTING BUS STOP				



**GENERAL NOTES:**  
THE LIMITS OF THE RIGHT-OF-WAY IMPROVEMENTS FOR DR. MARTIN LUTHER KING JUNIOR BOULEVARD ARE LIMITED TO SAID RIGHT-OF-WAY FROM NW 12TH AVENUE TO THE INTERSECTION WITH INTERSTATE 95.  
THE SCOPE OF THIS WORK INCLUDES ELEMENTS FROM THE BACK OF CURB TO THE RIGHT-OF-WAY. THESE ELEMENTS INCLUDE: PEDESTRIAN LIGHT POLES, SIDEWALK SURFACE TREATMENT, MEDIAN LANDSCAPE, CROSSWALK DESIGN, PEDESTRIAN RAMP DESIGN, SITE FURNITURE SELECTION.  
THIS PROJECT DOES NOT INCLUDE THE FOLLOWING: ANY TRAVEL LANE ASSOCIATED WORK WITH THE EXCEPTION OF CROSSWALKS, IRRIGATION, LANDSCAPE UP-LIGHTING, CURB MODIFICATIONS, GRADING, DRAINAGE, AND UTILITIES.  
BASE INFORMATION FOR THIS DOCUMENT IS BASED ON AVAILABLE DATA, WHICH INCLUDES MIAMI-DADE COUNTY ROADWAY PLANS, AERIALS, AND FIELD OBSERVATION.  
THE EXACT LOCATIONS OF DRIVEWAYS ARE SUBJECT TO CHANGE BASED ON THE UPDATED SURVEY.

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS



**NOTE:**

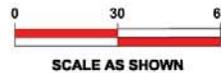
REFER TO THE STREETSCAPE ELEMENTS LIST FOLLOWING THE TECHNICAL LAYOUT FOR SPECIFIC STREETSCAPE MATERIALS AND SPECIFICATIONS.

**LEGEND:**

- RIGHT OF WAY
- EXISTING EDGE OF PAVEMENT
- SIDEWALK ZONE
- LANDSCAPE AREA
- DECORATIVE CORNER TREATMENT
- EXISTING BUS STOP

- EXISTING SIGNALIZED INTERSECTION
- LINEAR PARK LOCATION
- EXISTING AND PROPOSED DRIVEWAYS
- PROPOSED PEDESTRIAN LIGHT POLES
- PROPOSED GATEWAY FEATURE

- PROPOSED CANOPY TREES
- PROPOSED FLOWERING TREES
- PROPOSED PALMS
- EXISTING CANOPY TREE
- EXISTING PALMS



**GENERAL NOTES:**

THE LIMITS OF THE RIGHT-OF-WAY IMPROVEMENTS FOR DR. MARTIN LUTHER KING JUNIOR BOULEVARD ARE LIMITED TO SAID RIGHT-OF-WAY FROM NW 12TH AVENUE TO THE INTERSECTION WITH INTERSTATE 95.

THE SCOPE OF THIS WORK INCLUDES ELEMENTS FROM THE BACK OF CURB TO THE RIGHT-OF-WAY. THESE ELEMENTS INCLUDE: PEDESTRIAN LIGHT POLES, SIDEWALK SURFACE TREATMENT, MEDIAN LANDSCAPE, CROSSWALK DESIGN, PEDESTRIAN RAMP DESIGN, SITE FURNITURE SELECTION.

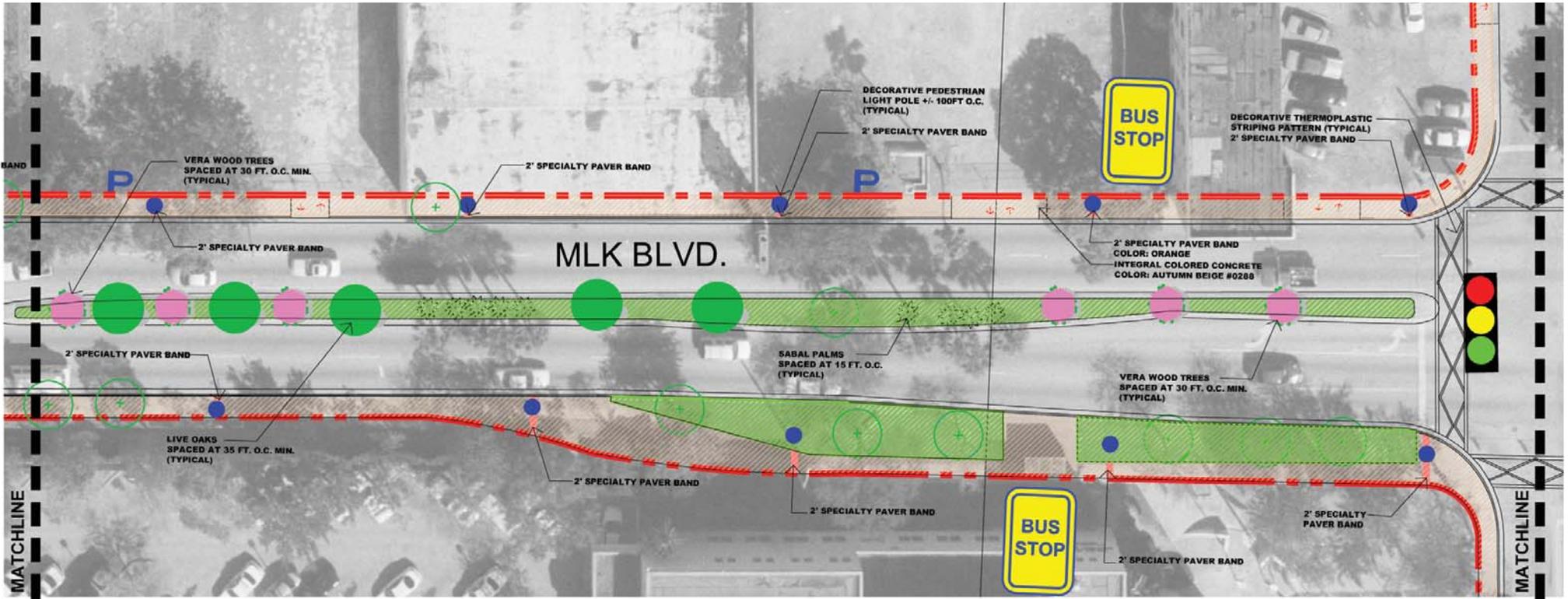
THIS PROJECT DOES NOT INCLUDE THE FOLLOWING: ANY TRAVEL LANE ASSOCIATED WORK WITH THE EXCEPTION OF CROSSWALKS, IRRIGATION, LANDSCAPE UP-LIGHTING, CURB MODIFICATIONS, GRADING, DRAINAGE, AND UTILITIES.

BASE INFORMATION FOR THIS DOCUMENT IS BASED ON AVAILABLE DATA, WHICH INCLUDES MIAM-DADE COUNTY ROADWAY PLANS, AERIALS, AND FIELD OBSERVATION.

THE EXACT LOCATIONS OF DRIVEWAYS ARE SUBJECT TO CHANGE BASED ON THE UPDATED SURVEY.



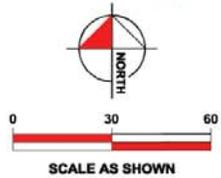
# DR. MARTIN LUTHER KING, JR. BOULEVARD



**NOTE:**

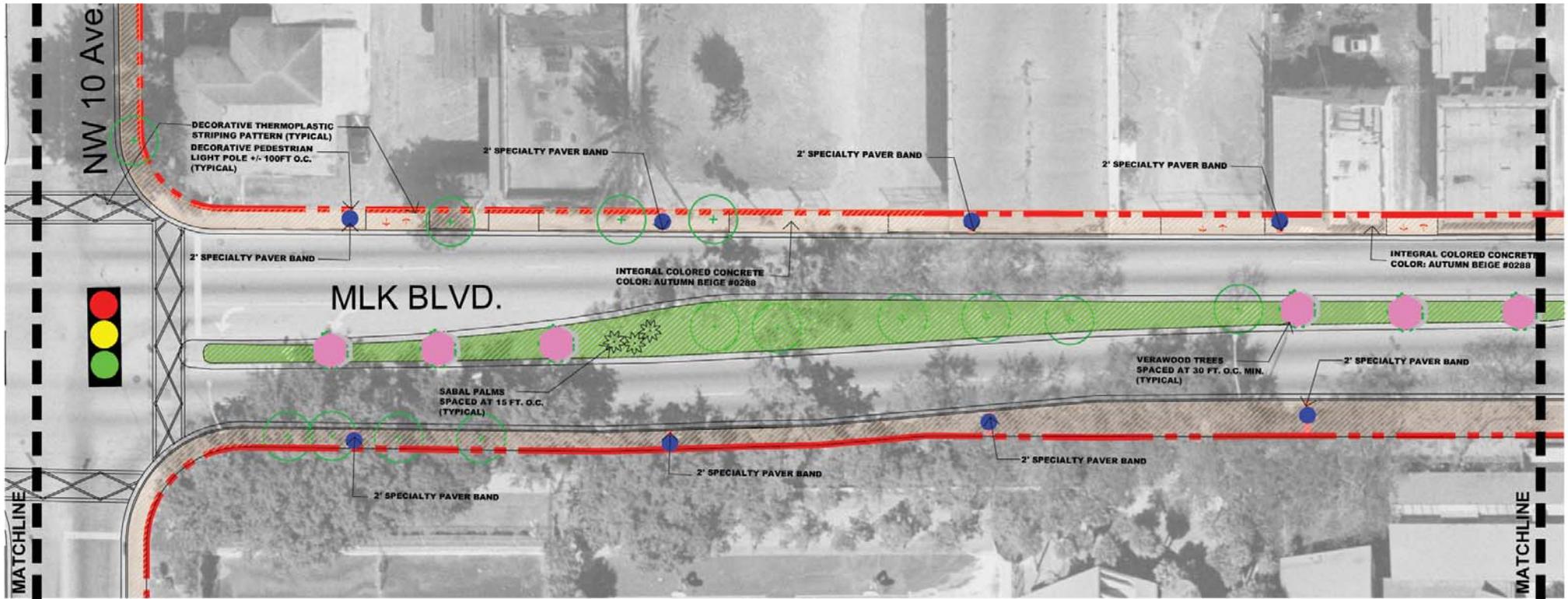
REFER TO THE STREETScape ELEMENTS LIST FOLLOWING THE TECHNICAL LAYOUT FOR SPECIFIC STREETScape MATERIALS AND SPECIFICATIONS.

- LEGEND:**
- RIGHT OF WAY
  - EXISTING EDGE OF PAVEMENT
  - SIDEWALK ZONE
  - LANDSCAPE AREA
  - DECORATIVE CORNER TREATMENT
  - EXISTING BUS STOP
  - EXISTING SIGNALIZED INTERSECTION
  - LINEAR PARK LOCATION
  - EXISTING AND PROPOSED DRIVEWAYS
  - PROPOSED PEDESTRIAN LIGHT POLES
  - PROPOSED GATEWAY FEATURE
  - PROPOSED CANOPY TREES
  - PROPOSED FLOWERING TREES
  - PROPOSED PALMS
  - EXISTING CANOPY TREE
  - EXISTING PALMS

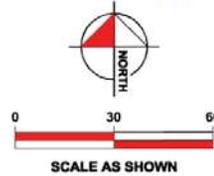
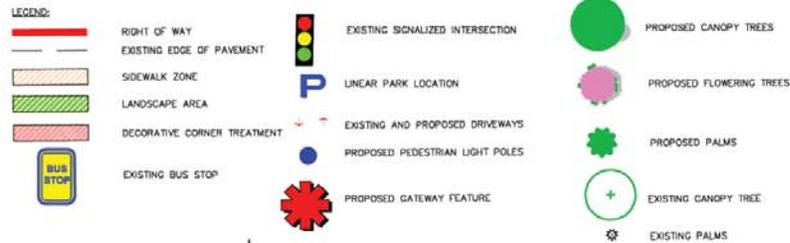


**GENERAL NOTES:**  
 THE LIMITS OF THE RIGHT-OF-WAY IMPROVEMENTS FOR DR. MARTIN LUTHER KING, JR. BOULEVARD ARE LIMITED TO SAID RIGHT-OF-WAY FROM NW 12TH AVENUE TO THE INTERSECTION WITH INTERSTATE 95.  
 THE SCOPE OF THIS WORK INCLUDES ELEMENTS FROM THE BACK OF CURB TO THE RIGHT-OF-WAY. THESE ELEMENTS INCLUDE: PEDESTRIAN LIGHT POLES, SIDEWALK SURFACE TREATMENT, MEDIAN LANDSCAPE, CROSSWALK DESIGN, PEDESTRIAN RAMP DESIGN, SITE FURNITURE SELECTION.  
 THIS PROJECT DOES NOT INCLUDE THE FOLLOWING: ANY TRAVEL LANE ASSOCIATED WORK WITH THE EXCEPTION OF CROSSWALKS, IRRIGATION, LANDSCAPE UP-LIGHTING, CURB MODIFICATIONS, GRADING, DRAINAGE, AND UTILITIES.  
 BASE INFORMATION FOR THIS DOCUMENT IS BASED ON AVAILABLE DATA, WHICH INCLUDES MIAMI-DADE COUNTY ROADWAY PLANS, AERIALS, AND FIELD OBSERVATION.  
 THE EXACT LOCATIONS OF DRIVEWAYS ARE SUBJECT TO CHANGE BASED ON THE UPDATED SURVEY.

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS



**NOTE:**  
REFER TO THE STREETSCAPE ELEMENTS LIST FOLLOWING THE TECHNICAL LAYOUT FOR SPECIFIC STREETSCAPE MATERIALS AND SPECIFICATIONS.



**GENERAL NOTES:**  
THE LIMITS OF THE RIGHT-OF-WAY IMPROVEMENTS FOR DR. MARTIN LUTHER KING JUNIOR BOULEVARD ARE LIMITED TO SAID RIGHT-OF-WAY FROM NW 12TH AVENUE TO THE INTERSECTION WITH INTERSTATE 95.  
THE SCOPE OF THIS WORK INCLUDES ELEMENTS FROM THE BACK OF CURB TO THE RIGHT-OF-WAY. THESE ELEMENTS INCLUDE: PEDESTRIAN LIGHT POLES, SIDEWALK SURFACE TREATMENT, MEDIAN LANDSCAPE, CROSSWALK DESIGN, PEDESTRIAN RAMP DESIGN, SITE FURNITURE SELECTION.  
THIS PROJECT DOES NOT INCLUDE THE FOLLOWING: ANY TRAVEL LANE ASSOCIATED WORK WITH THE EXCEPTION OF CROSSWALKS, IRRIGATION, LANDSCAPE UP-LIGHTING, CURB MODIFICATIONS, GRADING, DRAINAGE, AND UTILITIES.  
BASE INFORMATION FOR THIS DOCUMENT IS BASED ON AVAILABLE DATA, WHICH INCLUDES MIAMI-DADE COUNTY ROADWAY PLANS, AERIALS, AND FIELD OBSERVATION.  
THE EXACT LOCATIONS OF DRIVEWAYS ARE SUBJECT TO CHANGE BASED ON THE UPDATED SURVEY.



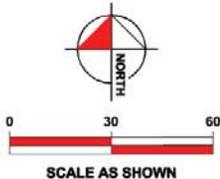
# DR. MARTIN LUTHER KING, JR. BOULEVARD



**NOTE:**  
REFER TO THE STREETSCAPE ELEMENTS LIST FOLLOWING THE TECHNICAL LAYOUT FOR SPECIFIC STREETSCAPE MATERIALS AND SPECIFICATIONS.

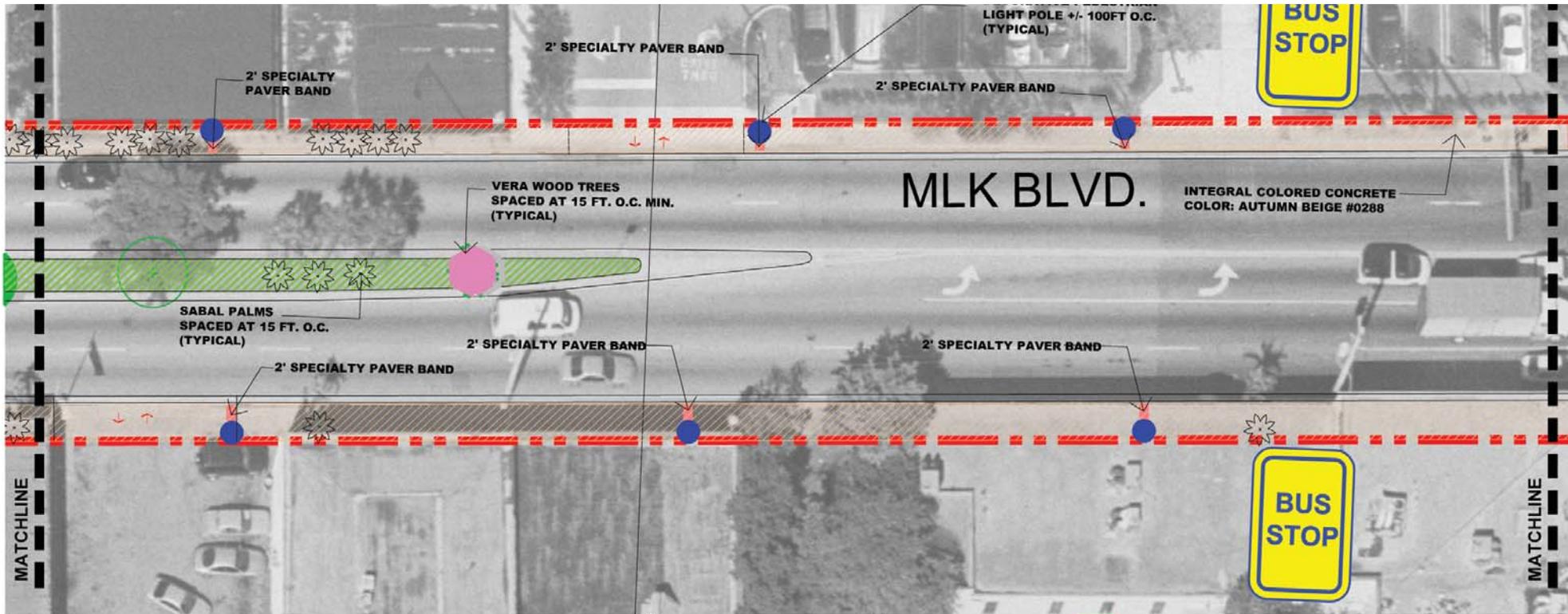
**LEGEND:**

- RIGHT OF WAY
- EXISTING EDGE OF PAVEMENT
- SIDEWALK ZONE
- LANDSCAPE AREA
- DECORATIVE CORNER TREATMENT
- EXISTING BUS STOP
- EXISTING SIGNALIZED INTERSECTION
- LINEAR PARK LOCATION
- EXISTING AND PROPOSED DRIVEWAYS
- PROPOSED PEDESTRIAN LIGHT POLES
- PROPOSED GATEWAY FEATURE
- PROPOSED CANOPY TREES
- PROPOSED FLOWERING TREES
- PROPOSED PALMS
- EXISTING CANOPY TREE
- EXISTING PALMS

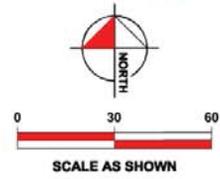


**GENERAL NOTES:**  
THE LIMITS OF THE RIGHT-OF-WAY IMPROVEMENTS FOR DR. MARTIN LUTHER KING JUNIOR BOULEVARD ARE LIMITED TO SAID RIGHT-OF-WAY FROM NW 12TH AVENUE TO THE INTERSECTION WITH INTERSTATE 95.  
THE SCOPE OF THIS WORK INCLUDES ELEMENTS FROM THE BACK OF CURB TO THE RIGHT-OF-WAY. THESE ELEMENTS INCLUDE: PEDESTRIAN LIGHT POLES, SIDEWALK SURFACE TREATMENT, MEDIAN LANDSCAPE, CROSSWALK DESIGN, PEDESTRIAN RAMP DESIGN, SITE FURNITURE SELECTION.  
THIS PROJECT DOES NOT INCLUDE THE FOLLOWING: ANY TRAVEL LANE ASSOCIATED WORK WITH THE EXCEPTION OF CROSSWALKS, IRRIGATION, LANDSCAPE UP-LIGHTING, CURB MODIFICATIONS, GRADING, DRAINAGE, AND UTILITIES.  
BASE INFORMATION FOR THIS DOCUMENT IS BASED ON AVAILABLE DATA, WHICH INCLUDES MIAMI-DADE COUNTY ROADWAY PLANS, AERIALS, AND FIELD OBSERVATION.  
THE EXACT LOCATIONS OF DRIVEWAYS ARE SUBJECT TO CHANGE BASED ON THE UPDATED SURVEY.

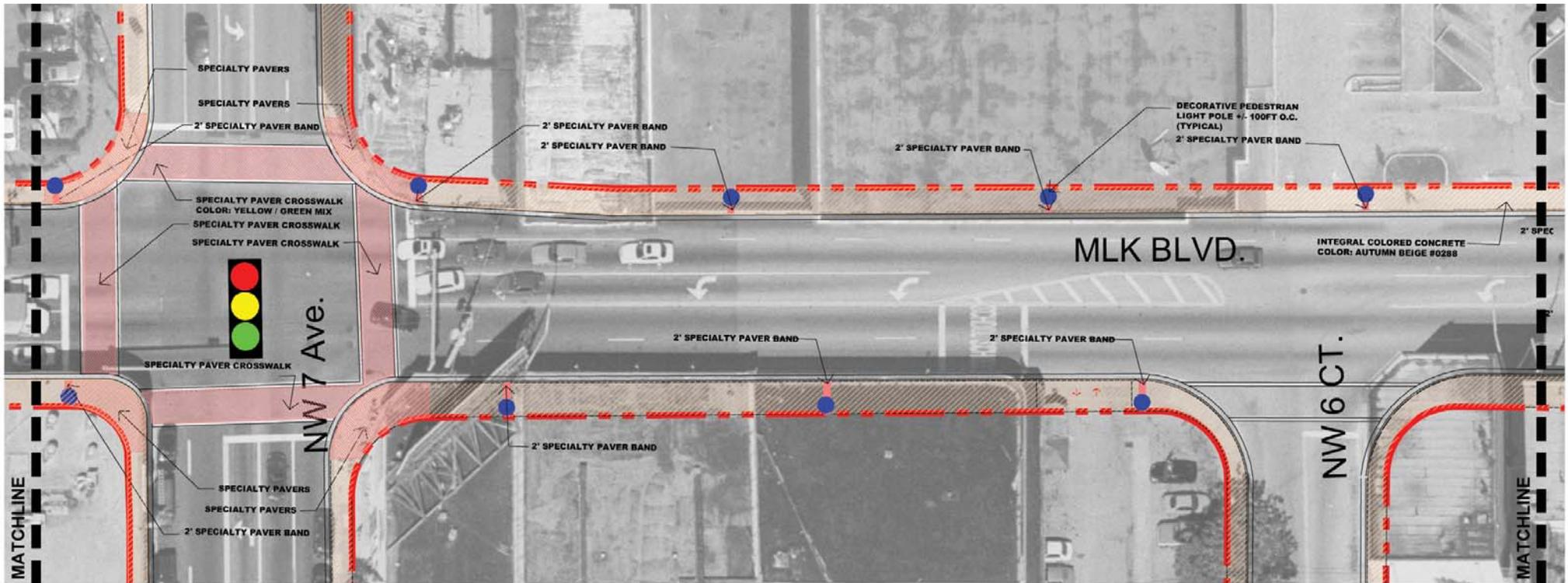
# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS



**NOTE:**  
REFER TO THE STREETSCAPE ELEMENTS LIST FOLLOWING THE TECHNICAL LAYOUT FOR SPECIFIC STREETSCAPE MATERIALS AND SPECIFICATIONS.



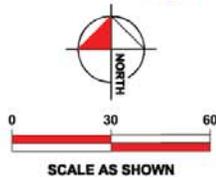
**GENERAL NOTES:**  
THE LIMITS OF THE RIGHT-OF-WAY IMPROVEMENTS FOR DR. MARTIN LUTHER KING JUNIOR BOULEVARD ARE LIMITED TO SAID RIGHT-OF-WAY FROM NW 12TH AVENUE TO THE INTERSECTION WITH INTERSTATE 95.  
THE SCOPE OF THIS WORK INCLUDES ELEMENTS FROM THE BACK OF CURB TO THE RIGHT-OF-WAY. THESE ELEMENTS INCLUDE: PEDESTRIAN LIGHT POLES, SIDEWALK SURFACE TREATMENT, MEDIAN LANDSCAPE, CROSSWALK DESIGN, PEDESTRIAN RAMP DESIGN, SITE FURNITURE SELECTION.  
THIS PROJECT DOES NOT INCLUDE THE FOLLOWING: ANY TRAVEL LANE ASSOCIATED WORK WITH THE EXCEPTION OF CROSSWALKS, IRRIGATION, LANDSCAPE UP-LIGHTING, CURB MODIFICATIONS, GRADING, DRAINAGE, AND UTILITIES.  
BASE INFORMATION FOR THIS DOCUMENT IS BASED ON AVAILABLE DATA, WHICH INCLUDES MIAMI-DADE COUNTY ROADWAY PLANS, AERIALS, AND FIELD OBSERVATION.  
THE EXACT LOCATIONS OF DRIVEWAYS ARE SUBJECT TO CHANGE BASED ON THE UPDATED SURVEY.



**NOTE:**  
REFER TO THE STREETSCAPE ELEMENTS LIST FOLLOWING THE TECHNICAL LAYOUT FOR SPECIFIC STREETSCAPE MATERIALS AND SPECIFICATIONS.

**LEGEND:**

- |  |                             |  |                                  |  |                          |
|--|-----------------------------|--|----------------------------------|--|--------------------------|
|  | RIGHT OF WAY                |  | EXISTING SIGNALIZED INTERSECTION |  | PROPOSED CANOPY TREES    |
|  | EXISTING EDGE OF PAVEMENT   |  | LINEAR PARK LOCATION             |  | PROPOSED FLOWERING TREES |
|  | SIDEWALK ZONE               |  | EXISTING AND PROPOSED DRIVEWAYS  |  | PROPOSED PALMS           |
|  | LANDSCAPE AREA              |  | PROPOSED PEDESTRIAN LIGHT POLES  |  | EXISTING CANOPY TREE     |
|  | DECORATIVE CORNER TREATMENT |  | PROPOSED GATEWAY FEATURE         |  | EXISTING PALMS           |
|  | EXISTING BUS STOP           |  |                                  |  |                          |

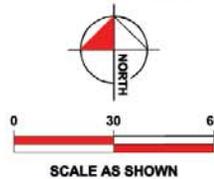
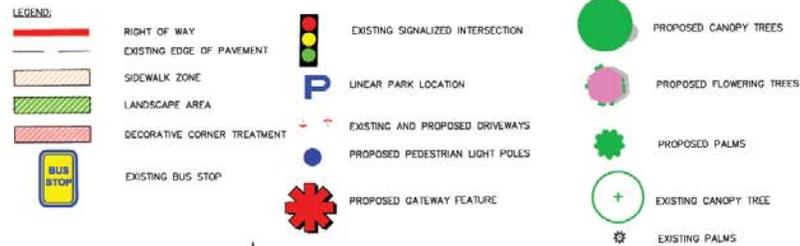


**GENERAL NOTES:**  
THE LIMITS OF THE RIGHT-OF-WAY IMPROVEMENTS FOR DR. MARTIN LUTHER KING JUNIOR BOULEVARD ARE LIMITED TO SAID RIGHT-OF-WAY FROM NW 12TH AVENUE TO THE INTERSECTION WITH INTERSTATE 95.  
THE SCOPE OF THIS WORK INCLUDES ELEMENTS FROM THE BACK OF CURB TO THE RIGHT-OF-WAY. THESE ELEMENTS INCLUDE: PEDESTRIAN LIGHT POLES, SIDEWALK SURFACE TREATMENT, MEDIAN LANDSCAPE, CROSSWALK DESIGN, PEDESTRIAN RAMP DESIGN, SITE FURNITURE SELECTION.  
THIS PROJECT DOES NOT INCLUDE THE FOLLOWING: ANY TRAVEL LANE ASSOCIATED WORK WITH THE EXCEPTION OF CROSSWALKS, IRRIGATION, LANDSCAPE UP-LIGHTING, CURB MODIFICATIONS, GRADING, DRAINAGE, AND UTILITIES.  
BASE INFORMATION FOR THIS DOCUMENT IS BASED ON AVAILABLE DATA, WHICH INCLUDES MIAMI-DADE COUNTY ROADWAY PLANS, AERIALS, AND FIELD OBSERVATION.  
THE EXACT LOCATIONS OF DRIVEWAYS ARE SUBJECT TO CHANGE BASED ON THE UPDATED SURVEY.

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS



**NOTE:**  
REFER TO THE STREETSCAPE ELEMENTS LIST FOLLOWING THE TECHNICAL LAYOUT FOR SPECIFIC STREETSCAPE MATERIALS AND SPECIFICATIONS.



**GENERAL NOTES:**  
THE LIMITS OF THE RIGHT-OF-WAY IMPROVEMENTS FOR DR. MARTIN LUTHER KING JUNIOR BOULEVARD ARE LIMITED TO SAID RIGHT-OF-WAY FROM NW 12TH AVENUE TO THE INTERSECTION WITH INTERSTATE 95.

THE SCOPE OF THIS WORK INCLUDES ELEMENTS FROM THE BACK OF CURB TO THE RIGHT-OF-WAY. THESE ELEMENTS INCLUDE: PEDESTRIAN LIGHT POLES, SIDEWALK SURFACE TREATMENT, MEDIAN LANDSCAPE, CROSSWALK DESIGN, PEDESTRIAN RAMP DESIGN, SITE FURNITURE SELECTION.

THIS PROJECT DOES NOT INCLUDE THE FOLLOWING: ANY TRAVEL LANE ASSOCIATED WORK WITH THE EXCEPTION OF CROSSWALKS, IRRIGATION, LANDSCAPE UP-LIGHTING, CURB MODIFICATIONS, GRADING, DRAINAGE, AND UTILITIES.

BASE INFORMATION FOR THIS DOCUMENT IS BASED ON AVAILABLE DATA, WHICH INCLUDES MIAMI-DADE COUNTY ROADWAY PLANS, AERIALS, AND FIELD OBSERVATION.

THE EXACT LOCATIONS OF DRIVEWAYS ARE SUBJECT TO CHANGE BASED ON THE UPDATED SURVEY.

**S T R E E T S C A P E   E L E M E N T S**



# DR. MARTIN LUTHER KING, JR. BOULEVARD

## S T R E E T S C A P E E L E M E N T S

Accent Plant 	SCIENTIFIC NAME	COMMON NAME	OVERALL HEIGHT
	Bromeliad	Bromeliad varieties	3-5'
	Crinum x amabile	Crinum Lily	4-6'
	Alpinia zerumbet	Shell Ginger	3-5'

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

Shrubs 	SCIENTIFIC NAME	COMMON NAME	OVERALL HEIGHT
	Raphiolepis indica	Indian Hawthorn	2'-2 1/2'
	Jasminum multiflorum	Jasmine	2'-3'
	Chrysobalanus icaco	Cocoplum	2'-4'

Note: The palette of plants presented in this document constitutes xeric and/or native plants that require minimal maintenance.



# DR. MARTIN LUTHER KING, JR. BOULEVARD

Shrubs 	SCIENTIFIC NAME	COMMON NAME	OVERALL HEIGHT
	Schefflera arboricola 'Trinette'	Dwarf Variegated Schefflera	4'-8'
 <small>©2001 Hortiopia, Inc.</small>	Rapanea guinensis	Florida Myrsine	4'-8'

Note: The palette of plants presented in this document constitutes xeric and/or native plants that require minimal maintenance.

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

Flowering Tree 	<b>SCIENTIFIC NAME</b>	<b>COMMON NAME</b>	<b>OVERALL HEIGHT</b>	<b>HEIGHT AT TIME OF PLANTING</b>
	Lagerstroemia indica	Crape Myrtle	15-20'	8'
	Bulnesia	Verawood	to 15'	8'

Note: The palette of plants presented in this document constitutes xeric and/or native plants that require minimal maintenance.



# DR. MARTIN LUTHER KING, JR. BOULEVARD

Large Canopy Tree 	SCIENTIFIC NAME	COMMON NAME	OVERALL HEIGHT	HEIGHT AT TIME OF PLANTING
	Quercus virginiana	Live Oak	40-60'	12-16'

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

Large Palm 	SCIENTIFIC NAME	COMMON NAME	OVERALL HEIGHT	HEIGHT AT TIME OF PLANTING
	Washingtonia robusta	Washington Palm	20-30'	14-18'
	Sabal palmetto	Cabbage Palm	20-40'	14-18'

Note: The palette of plants presented in this document constitutes xeric and/or native plants that require minimal maintenance.

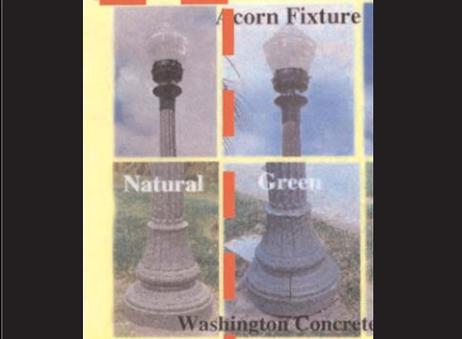


# DR. MARTIN LUTHER KING, JR. BOULEVARD

Small Palm 	SCIENTIFIC NAME	COMMON NAME	OVERALL HEIGHT	HEIGHT AT TIME OF PLANTING
	Veitchia montgomeryana	Arecaceae Palm	20-30'	10-14'

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

## STREET FURNITURE PALETTE

	Furniture type/ Name	Manufacturer	Color/ Specs.
	<p>City Bench with Dr. Martin Luther King, Jr. Emblem</p>	<p>City Standard</p>	<p>Black</p>
	<p>City Newsrack</p>	<p>City Standard</p>	<p>Black</p>
	<p>Decorative Light Pole (Pedestrian) - 14' Mounting Height</p>	<p>Ameron (FPL Decorative Package)</p>	<p>Fixture - Black Pole - Dark Grey</p>

Note: Martin Luther King, Jr. image shall be one of the four(4) images licensed to the MLK EDC for use by the King estate.



# DR. MARTIN LUTHER KING, JR. BOULEVARD

	<b>Furniture type/ Name</b>	<b>Manufacturer</b>	<b>Color/ Specs.</b>
 <p>designed by Robert Dippman, ASLA P.C. Patent No. 7,172,410</p>	Pi Bike Rack	Landscapeforms	Black
 <p>designed by Arno Yurk, AIA, IDSA</p>	Scarborough Trash Receptacle	Landscapeforms	Black

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

## H A R D S C A P E P A L E T T E

	Hardscape Type	Manufacturer	Color/ Specs.
	Thermoplastic Striping Crosswalk	Varies	White - 4" thick
	Integral Color Concrete Sidewalk	Scofield or approved equal	Scofield color #0288 'Autumn Beige' 
	Brick Pavers	Varies	Varies

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

## OPINION OF PROBABLE CONSTRUCTION COST

\*\*

Description	Quantity	Unit	Unit Price	Cost
<b>ROADWAY/HARDSCAPE</b>				
Driveways (Urban Flare)	34	SY	\$60	\$2,040
2' Detectable Warning Domes (ADA ramps)	350	LF	\$25	\$8,750
Color concrete sidewalk (4"slab) Includes Demolition	9480	SY	\$70	\$663,600
Decorative Paver (Corner Treatment)	470	SY	\$70	\$32,900
Decorative Paver (2' sidewalk bands)	112	SY	\$70	\$7,840
Decorative Paver Crosswalks (includes base treatment and Conc. Header)	330	SY	\$90	\$29,700
Linear Parks **	6	EA	\$15,000.00	\$90,000
<i>Subtotal</i>				\$834,830
<b>SIGNING AND MARKING</b>				
Solid traffic stripe, thermoplastic [6"] [white]	3200	LF	\$1.50	\$4,800
<i>Subtotal</i>				\$4,800
<b>LANDSCAPE</b>				
Vera Wood	25	EA	\$350	\$8,750
Live Oak	15	EA	\$600	\$9,000
Small Palm	20	EA	\$120	\$2,400
Groundcover	5000	SF	\$1	\$5,000
Shrubs	2500	SF	\$3	\$7,500
Accent Shrub	100	EA	\$18	\$1,800
Bahia Sod (Paspalum spp.)	6,000	SF	\$0.15	\$900
<i>Subtotal</i>				\$35,350
<b>STREET FURNITURE</b>				
Benches	8	EA	\$1,200	\$9,600
Bicycle Racks	4	EA	\$600	\$2,400
Trash Receptacles	10	EA	\$800	\$8,000
Pedestrian Light Poles	66	EA	\$3,500	\$231,000
<i>Subtotal</i>				\$251,000
<b>SUBTOTAL</b>				<b>\$1,125,980</b>
<i>Mobilization (10%)</i>				\$112,598
<i>Construction Contingency (10%)</i>				\$123,858
<b>TOTAL COSTS</b>				<b>\$1,362,436</b>

Since the Engineer has no control over time, labor and material cost furnished by others, or over methods of determining prices, or market conditions, all opinions rendered herein as to cost, represent its best judgement; the Engineer does not guarantee that actual cost will not vary from opinion of cost.

\*\*Note: Estimate does not include design and administrative fees



# DR. MARTIN LUTHER KING, JR. BOULEVARD

**F A C A D E   S T A N D A R D S**



## INTRODUCTION

### Private Sector Facade Standards

#### Background/Intent

This document is a continuation of the efforts that have already begun in the community to enhance the economic potential of the businesses within the MLK corridor. It is the intent of this document to establish clear and concise facade improvement guidelines from which designers can begin to improve the physical appearance of their business in hopes to increase output and better economic success throughout the corridor.

In addition, these standards establish a consistent theme and look for the facades, which enhance the character and identity of MLK Boulevard and create a more pleasant environment that encourages window shopping and attracts visitors and tourists. The African motifs and base colors selected in a public process for these guidelines will help establish the desired identity for the neighborhood as it experiences future redevelopment and growth.

While this document does not regulate uses, the intent is to maintain a consistency with the existing SD-1 ordinance in promoting active city streets. The physical standards set forth in this document should go hand in hand with active ground floor uses to generate the desired level of street activity.

Included in this section are the following:

- Facade improvement concept sketches
- Materials Standards
- Storefront Standards
- Articulation Standards
- Awning Standards
- Openings Standards
- Roof Standards
- Parking Standards
- Color Standards
- Signage Standards
- Frontage Standards



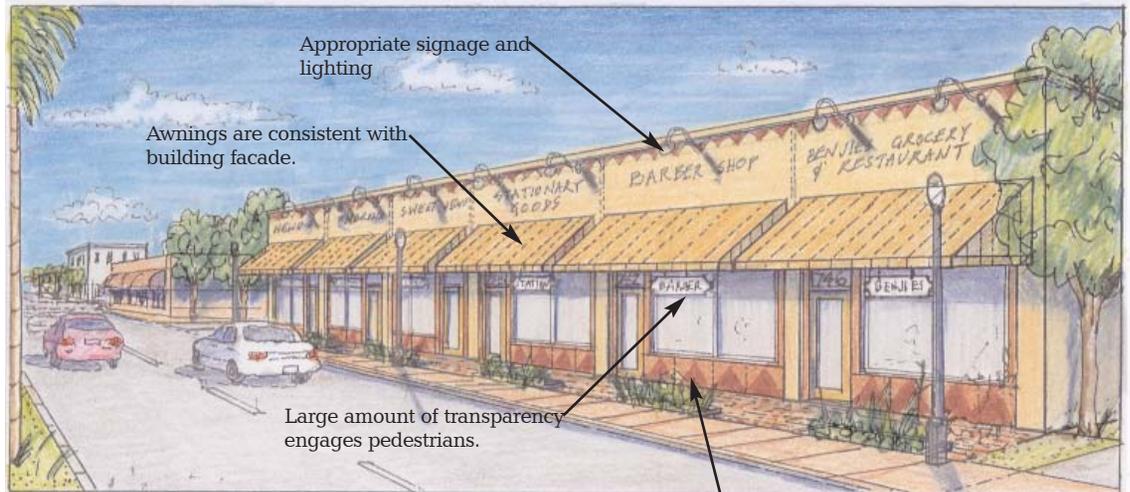
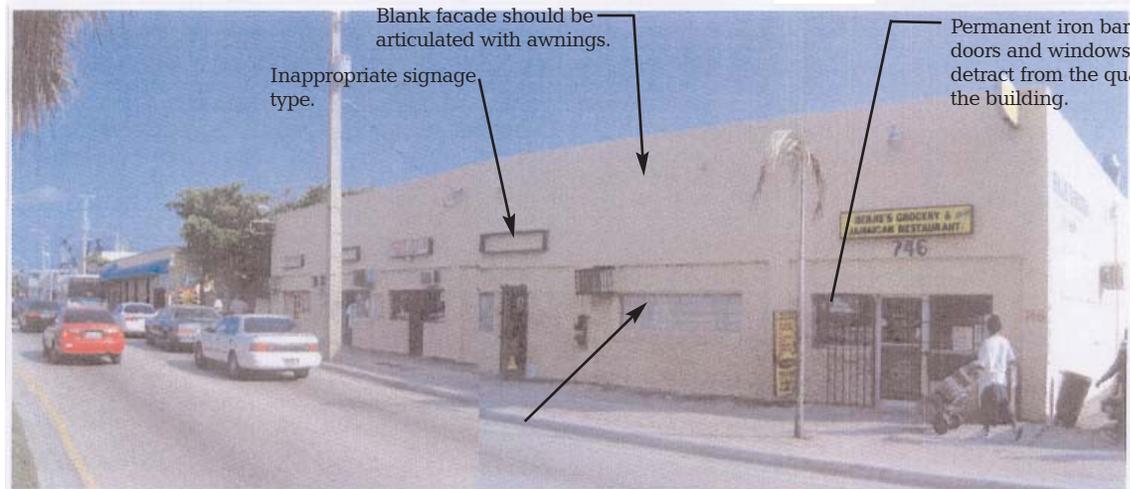
# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

## CONCEPT SKETCHES

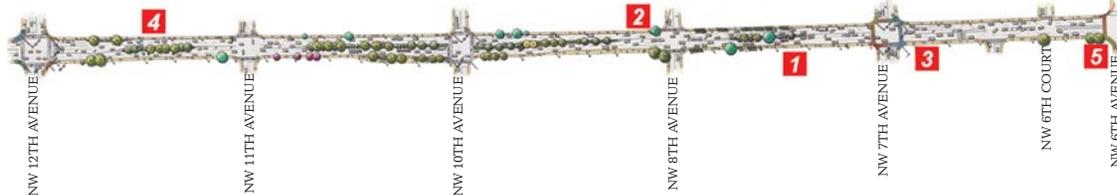
1

### Before and After Illustration

This image illustrates the potential for facade improvement should a property owner implement the guidelines presented in this document. The top image shows an existing commercial building along the MLK corridor. The lower image illustrates how the various facade elements such as awnings, windows, signage, lighting, etc. can dramatically enhance the appearance of a structure if a cohesive design language is utilized. The area between the facade and back of sidewalk is a critical area that can become a great asset to both the public and private sectors. Such areas can be used as seating areas for businesses, extension of the sidewalk and/or planting.



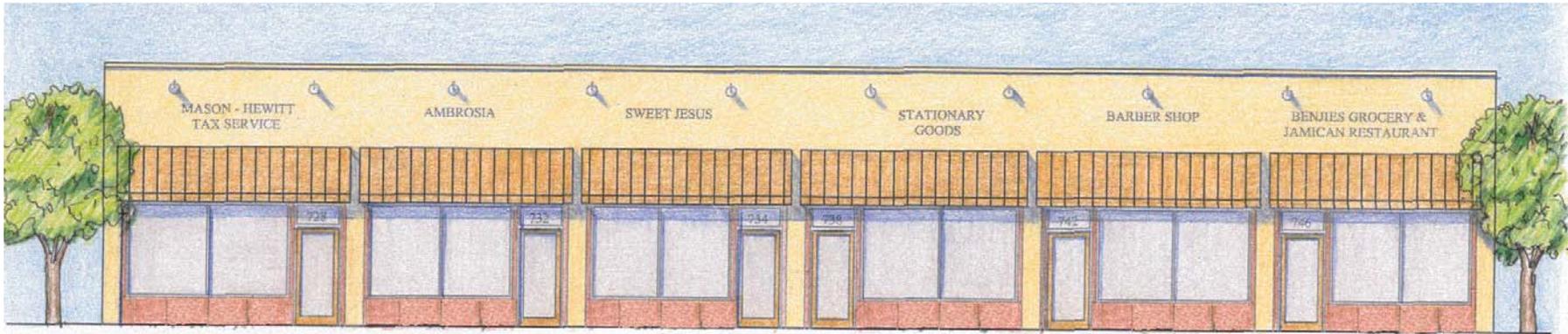
KEY MAP



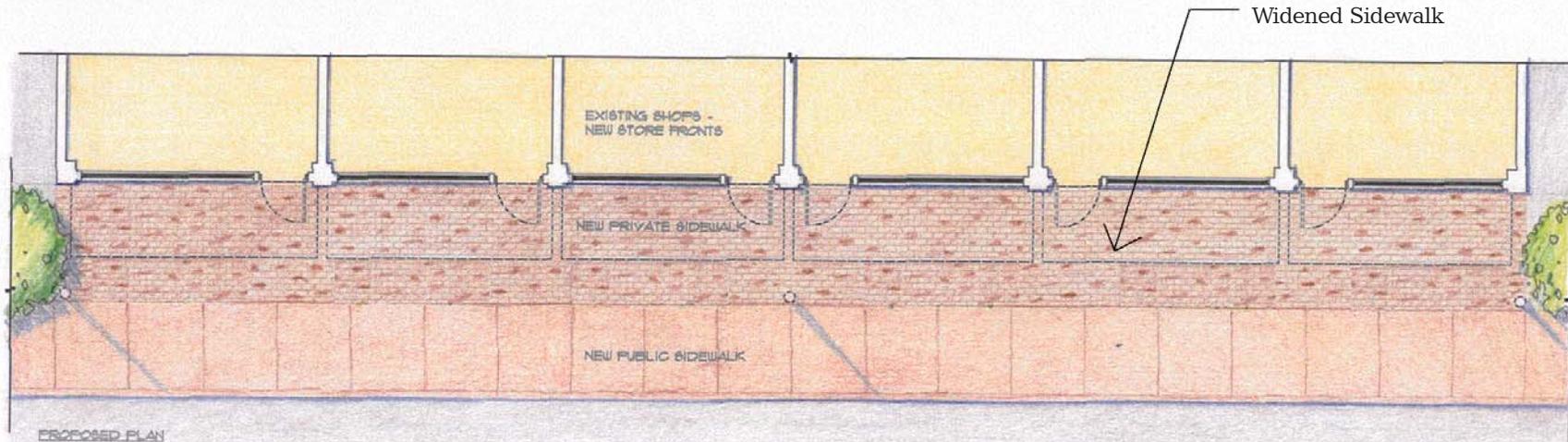


# DR. MARTIN LUTHER KING, JR. BOULEVARD

## Elevation and Plan View



PROPOSED FRONT ELEVATION  
MARCH 11, 2014 1:42



PROPOSED PLAN  
MARCH 11, 2014 1:42

MARTIN LUTHER KING BOULEVARD

Potential facade improvements. The plan view illustrates how the area in front of the facade, which is private property, can become a useful, well defined space that links the private property to the public corridor. In this particular case, the area may be used for outdoor seating, displays and gathering.

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

## Enlargement Diagrams



In order for the facade to reach its fullest potential, all elements should relate to each other as shown above. This provides for unity and cohesiveness in the design, and strengthens the intended character of the overall neighborhood.



# DR. MARTIN LUTHER KING, JR. BOULEVARD

## Potential Facade Improvement Scenarios

### Scenario 1 - Pallbearer's Hall

2



Existing Conditions



## Potential Facade Improvement Scenarios

### Scenario 1 - Pallbearer's Hall

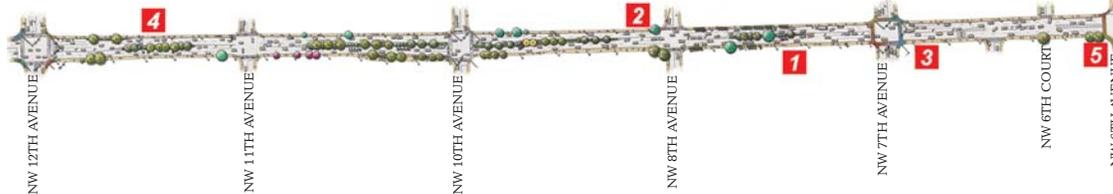
2

- Replace Windows
- Add Decorative Landscaping
- Repaint
- Repair concrete eyebrows



Potential Improvements (see list on left)

KEY MAP





# DR. MARTIN LUTHER KING, JR. BOULEVARD

## Potential Facade Improvement Scenarios

### Scenario 2 - The St. John's Building

3



Existing Conditions

KEY MAP



# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

## Potential Facade Improvement Scenarios

### Scenario 2 - The St. John's Building

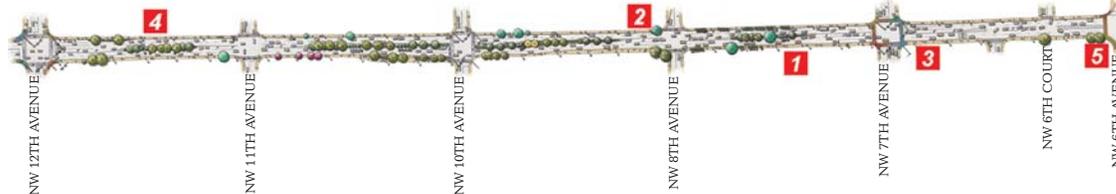
3

- Open up facade
- Add windows, doors, awnings and signage
- Remove stone from ground floor
- Repaint



Potential Improvements (see list on left)

KEY MAP





# DR. MARTIN LUTHER KING, JR. BOULEVARD

## Potential Facade Improvement Scenarios

### Scenario 3 - Yaeger Medical Building

4



Existing Conditions

KEY MAP



# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

## Potential Facade Improvement Scenarios

4

- Create front courtyard
- Open up front facade
- Repaint and touch up facade
- Standardize lighting fixtures



Potential Improvements (see list on left)





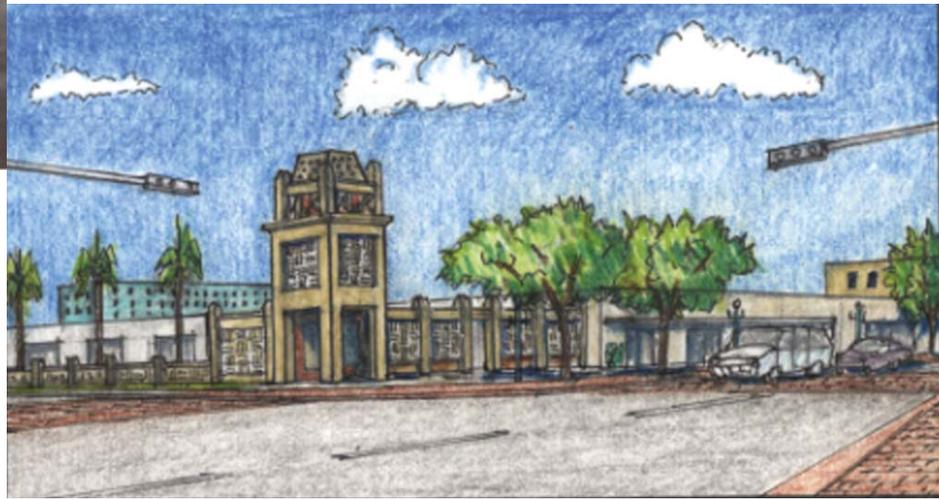
# DR. MARTIN LUTHER KING, JR. BOULEVARD

## Potential Gateway Elements MLK Boulevard and I-95

5



Existing conditions



Potential Gateway Element with African Motifs

KEY MAP



# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

## Potential Gateway Elements

### MLK Boulevard and I-95



Potential Gateway Element with African Motifs. This concept shown above illustrates a potential gateway scenario that will help create an identity for the corridor. Such identities will be a joint cooperation effort between the City and local property owners.



## Facade Standards

### General Standards

#### Background/Intent

The following standards serve to create a consistent appearance and treatment of the corridor. The facade standards allow for individuality while creating cohesive themes throughout the area. Based on an extensive public involvement process, an African based theme was developed for the various components in these standards. This theme transcends the facade portion of this plan and ties to the colors and patterns of the streetscape beautification to create a unified image for the corridor.

The facade design standards provide basic instructions for improvement of the various components that together constitute the facade of a building. By adhering to the standards, each individual building will be transformed into an integral part of the corridor, and in turn, the community.

It is the intent of this document to provide the standards by which designers and business owners can properly engage pedestrians and create opportunities for window shopping which will lead to a more vibrant corridor.

#### General Standards

- Encourage street frontage for new properties with minimum setbacks to engage streetscape and promote window shopping and interaction between public and private areas.
- All of the following standards are applicable along all street frontages.
- Promote unity and cohesive design with the Martin Luther King, Jr. streetscape beautification project.
- Enhance the African-based image of the community through use of color, articulation, and patterns.



Above is an example of a potential facade that adheres to the standards set forth herein.

## Facade Standards

### Frontage

#### Background/Intent

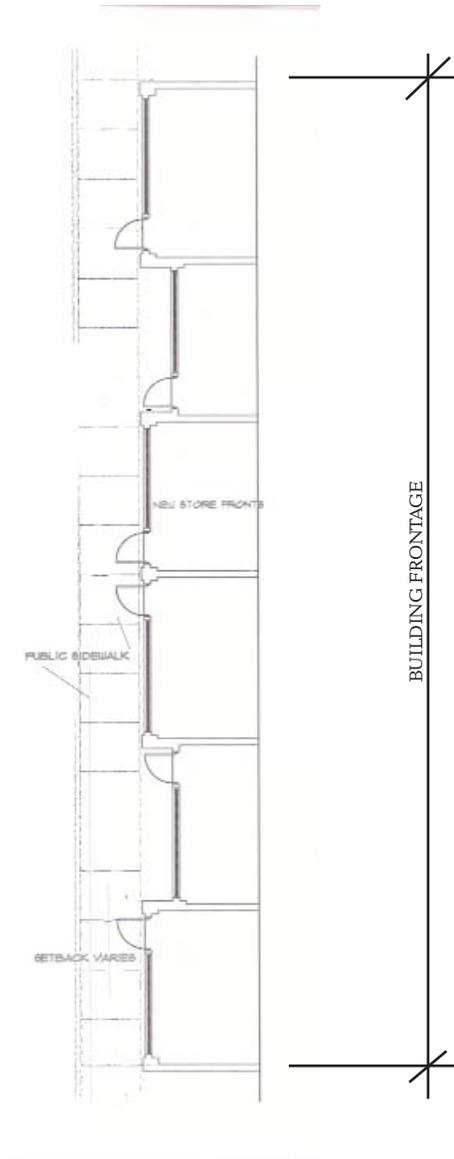
The purpose of establishing frontage requirements for MLK Boulevard is to create a vibrant and active street by encouraging interaction between the street and abutting uses. **Frontage is defined as active uses that front the street**. This may include cafe seating, restaurants, retail stores, etc.

#### Standard

**A minimum of 80% active building frontage shall be established for buildings abutting MLK Boulevard.**

This can be accomplished using the following criteria:

- All abutting properties shall have the main entrance along MLK Boulevard.
- Where garages or other non-active uses are required and cannot be moved to the side, they must be screened properly according to these guidelines.
- Vehicular access shall be limited where the building fronts the street. Vehicular traffic shall be redirected to the rear of the building.
- No parking shall be permitted between the building and the private property fronting a street.
- Parking structures shall be lined with active uses along street frontages.
- Blank walls shall constitute a maximum of 20% of the facade fronting MLK Boulevard.
- For multi-story buildings, the first three(3) stories shall engage the street with their uses.



Frontage Requirements

## Facade Standards

### Transition Zone

#### Background/Intent

Because buildings along the corridor have various setbacks from the street, the space between the right-of-way and the facade becomes an important area which can greatly contribute to the quality of the building. The intent of this document is to establish minimum standards to regulate the elements that make up this zone (refer to page 25 - Public/Private Relationships).

#### Standard

**The transition zone shall be defined as the horizontal space located between the right-of-way and the building facade. This space shall be utilized as either a landscape buffer, a seating area, or an extension of the public sidewalk.**

This can be accomplished using the following criteria:

- For landscape areas: Minimum landscape area width shall be five(5) feet. Landscaping shall be maintained by the property owner and kept in a healthy and vibrant condition. Potted plants and planters are allowed but shall not encroach into public sidewalk.
- For seating areas: A decorative hardscape treatment shall be placed along the transition zone. Refer to Hardscape Palette on page 52.
- No fixed seating or tables shall be permitted.
- Seating may encroach into public sidewalk with the provision that a minimum 5' clear zone is provided for pedestrian movement.
- Seating shall be set back a minimum of 3' from face of curb.
- For sidewalk extension areas: Such areas shall match sidewalk material, color, and texture.



The cafe seating shown above provides obstruction-free pedestrian passage through the seating area.



Planters are a good method to use when buffering the street from pedestrian accessways.

## Facade Standards

---

### Doors and Windows

#### Background/Intent

Openings are critical to the facade's architecture and its ability to engage the street. These standards focus on articulating window and door openings to enhance the architectural interest of the building.

#### Standard

**Facade openings shall be articulated to separate the plane of the building facade and create visual interest.**

This can be accomplished using the following criteria:

- Entrances shall be articulated with different building materials and changes of massing.
- Entrances shall be recessed a minimum of 6" from the building facade.
- Corner entrances shall be oriented to engage corner.
- Refer to transparency guideline for clear facade requirements.
- On masonry buildings, the lintels shall always be flush with the wall, while the sills shall project from the face of the building a minimum of one-half inch from the wall.
- The window heads and lintels shall be detailed differently than the facade, so that they have a similar stucco finish.
- The window sills of stucco finish buildings may be made of precast concrete or have a similar stucco finish.
- For multiple-story building, additional stories shall be

consistent in treatment with the design on the ground floor in materials, colors, and overall design.



This facade is simple, yet elegant. The openings are well-pronounced with the entryway set back a few inches from the facade and articulated with a different color.

## Facade Standards

### Roofs

#### Background/Intent

Roofs are a critical component of a building's appearance. Roofs in many ways can be one of the most dominating features of a building. While roof types vary greatly, the main roof variation in this area is the flat roof.

It is the intent of this document to enhance the appearance of the facades along MLK boulevard by establishing clear and concise standards for roof design.

#### Standard

**Roofs shall adhere to the standards set forth herein.**

This can be accomplished using the following criteria:

- Roofs shall be flat and shall be required to have a parapet above the roof facing any frontage. The parapet wall shall be a minimum of 12 inches tall (measured above the roof).
- Any equipment placed on a flat roof is required to be screened by parapet walls or other devices, in order to render them invisible from the street level.
- Roof penetrations of a mechanical nature (vents, pipes, ducts, etc) shall not be visible from the street level.
- Roof must be consistent with the style of architecture. Hybrid styles shall not be allowed (i.e. Modern building with barrel tile roof).
- Where architecturally appropriate, African motifs shall be incorporated into the cornice and/or parapet design of the building.
- Roofs shall be recessed and/or screened and not visible from the street level.



The parapet on the roof above is successful in hiding the rooftop mechanical equipment from the street level. African motifs are encouraged to be incorporated with the cornice to enhance the desired theme of the corridor.



Flat roofs with parapet walls such as the one shown in the image above provide opportunity for architectural articulation and uniqueness in the design.

## Facade Standards

### Facade Articulation

#### Background/Intent

Through the public process, it was determined that African motifs should be incorporated through the facade articulation in order to establish a meaningful theme throughout the corridor. These motifs/patterns should serve to highlight aspects of the facade but should not overwhelm the Miami vernacular base of the building. There are numerous African-based patterns and motifs beyond those shown as examples in this book that a designer may use as inspiration for a particular design. There are also various opportunities along a facade where these patterns and motifs may be appropriately articulated. It is the intent of these standards to establish a minimum criteria to ensure that each facade incorporates some of these elements to carry the desired theme consistently along MLK Boulevard.

It is important to note that existing historically significant buildings which have an established architectural style (i.e. Art Deco) do not have to meet the requirements of this standard.

#### Standard

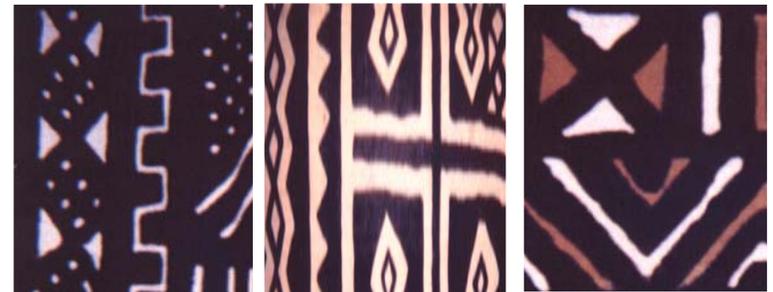
**All properties shall use at least one(1) acceptable African-based motif and/or pattern as part of the facade articulation.**

This can be accomplished using the following criteria:

- African motifs/pattern can be tiled or incorporated into the materials of the facade, unless it is a flat-roofed building.
- African motifs/patterns must be consistent throughout the building. Multiple patterns/motifs may be utilized if so desired with a consistent repetition.
- African motifs/patterns shall be used to highlight aspects of the facade and highlight the Miami vernacular of the building.
- For flat roofed buildings, a parapet wall with a decorative frieze incorporating African motifs shall be required.



Motifs combined with patterns articulated with concrete and tile.



African patterns that may serve as potential inspiration for facade articulation.



Rendering illustrating a potential articulation of a building facade with African-based patterns.



## Facade Standards

### Storefront

#### Background/Intent

MLK Boulevard has the potential to be an active business district with restaurants, boutiques, and other commercial amenities. The storefront appearance is one of the most important elements to the success of the boulevard. These standards strive to create active, attractive storefronts by enhancing the levels of transparency, regulating signage locations, openings, materials, awnings, and color. These elements, combined with African motifs, as part of the architectural articulation, will establish a meaningful theme that will continue throughout the corridor.

#### Standard

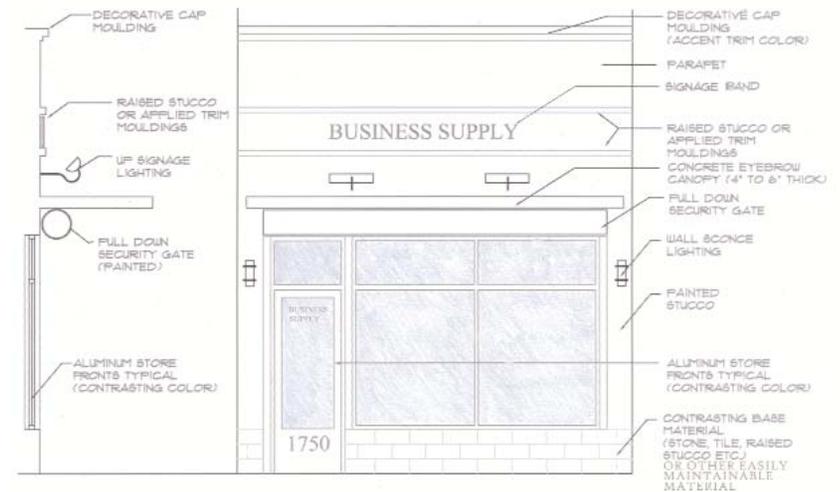
**For MLK Boulevard, storefronts must be transparent with windows and doors occupying a minimum of 70% of the ground floor linear frontage in the first 15' of the facade, measured vertically.**

This can be accomplished using the following criteria:

- Storefront materials shall allow unobstructed views to the inside of the establishment to promote "window shopping". Glass is encouraged as a storefront material.
- Doors shall be clear glass or of a decorative material.
- All glass used in storefronts shall be clear (not frosted, textured, or otherwise affected) and provide an unobstructed view of a minimum of six(6) feet into the establishment.
- Along side and rear facades, wall treatments shall be enhanced utilizing landscaping and/or architectural treatment.
- Black glass, Spandrel Glass, paper material or any other opaque material is prohibited.
- Art glass may be approved under special permit.
- Signage shall not obstruct the glass area.



The storefront above has excellent transparency with a low window base and large amounts of clear glass. All signage and clutter is kept free from the display windows.



ARCHITECTURAL ELEMENTS

Typical storefront elements

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

- Windows shall be set at a minimum of \*\*18 inches from the ground and within 12 inches of the first floor ceiling.
- Operable transom windows are encouraged above doors and storefront windows.
- Existing historically significant structures are not required to meet the 70% transparency requirement if it would impact the historic nature of the building.
- Roll-down security gates shall be incorporated into the design of the facade. Where possible, such gates shall be placed underneath the awning. Such gates, if solid shall have an ESP(Electrostatic Paint)finish to match building.
- High impact glass is encouraged over the use of security gates.
- Permanent bars and gates shall be prohibited.

\*\* Minimum of 18 inches may be increased if required to comply with flood criteria.



An attractive and successful storefront incorporates a variety of elements, such as consistent colors, high transparency ratio, attractive signage, lighting, materials, and colors.

## Facade Standards

### Materials

#### Background/Intent

Materials are an important element of the facade because they relate the building to its vernacular and time period. This is especially important along MLK Boulevard because there are several historically significant buildings which must be preserved as part of the living history of the neighborhood. In addition, materials also serve to enhance the desired African theme of the corridor's architecture. The intent of this document is to establish a palette of materials that gives the property owner a comfortable degree of choice while ensuring that a minimum standard of quality and character is established consistently throughout the corridor.

#### Standard

**At least two(2) different materials shall be used in each building elevation. When used for windows, glass shall not be counted as one of the materials.**

This can be accomplished using the following criteria:

- Acceptable materials should be indicative of Miami's vernacular and context. Such materials include, but are not limited to: stucco, coquina stone, precast concrete, tile, metal, glass, etc.
- Different materials shall be used to emphasize openings and entrances such as windows and doorways.
- Creative and innovative uses of materials and high quality building materials are encouraged.
- Materials should be easily maintainable and cleaned. Materials that collect debris(i.e. Coquina Stone) should be limited.



This county building located on NW 62nd Street and 27th Avenue utilizes vernacular material to represent African motifs, thus enhancing the desired theme of the corridor.



Glass



Coquina Stone \*\*



Ceramic Tile



Stucco



Metal Finishes

Above are materials that are permitted and encouraged along Martin Luther King Boulevard.

\*\*Suggested for use at higher levels.

## Facade Standards

### Identification Markers

#### Background/Intent

The purpose of identification markers is to emphasize the theme and character of the African American heritage and the connection of Dr. Martin Luther King, Jr. to the neighborhood. Appropriate information to be placed on these plaques may include: person, place, activity, event that highlights a significant part of the corridor.

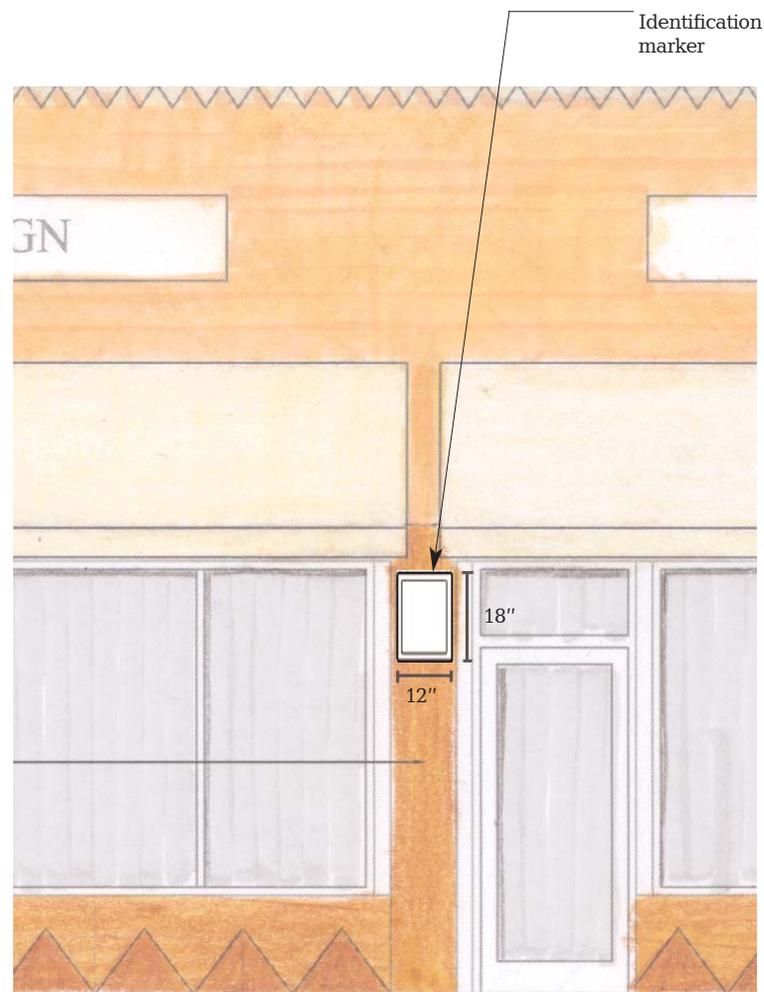
For marker designations, coordinate with the City of Miami.

#### Standard

**Business owners shall have the option of adding identification markers to the building facade as set forth herein.**

This can be accomplished using the following criteria:

- Identification markers shall be placed on the building facade facing MLK Boulevard at pedestrian eye level.
- Identification markers shall measure 12" width by 18" in height.
- Identification markers shall have a standard base color(as listed in this document). Lettering shall be 1/2" in height - Times New Roman font, black in color.
- Markers shall have a 1/2" border.
- There shall be a limit of one(1) identification marker per building.
- Identification markers shall be permanently affixed to a masonry part of the facade.





## Facade Standards

### Color

#### Background/Intent

The color that one paints his home or his business should be a personal decision. However, sometimes certain colors that are not generally seen on the exterior of buildings can create a negative perception of that house or business and its surrounding neighborhood. Having an approved color palette allows a certain freedom to the homeowner or business owner to choose his own color, but also to be consistent with his neighbors, whether they are neighboring homes or businesses.

#### Standard

**All buildings shall use one(1) of the following base colors(or approved equal). Said color shall constitute no less than 30% of the building facade along MLK boulevard. In addition to the base color, each building shall be allowed up to three(3) complementary colors for a total possible color variation of four(4) colors.**

This can be accomplished using the following criteria:

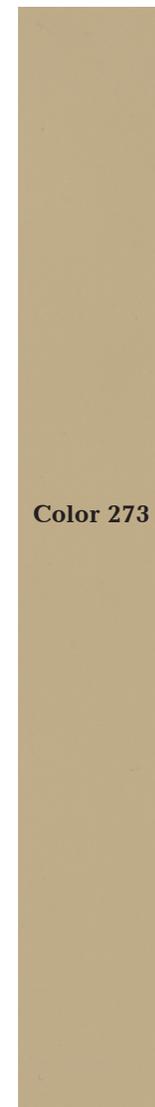
- Minimum of two(2) colors (including base color) shall be required per building.
- Base colors shall be Benjamin Moore #'s 1112, 1139, 273, and 151 or approved equal.
- Maximum of one(1) base color per facade.
- Awnings shall be one color throughout entire building.
- The color palette included herein represents the four(4) base colors selected for the facade improvements. Colors may vary from each base color by 3 hues(either darker or lighter).
- Colors shall not vary for different businesses within the same building.
- Paint colors shall reflect the historical age and/or style of the building, show the best features of the design, and represent the current owner's taste.



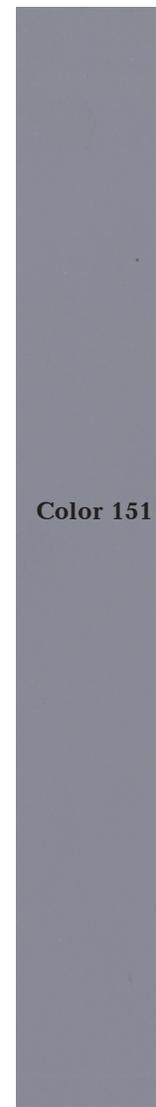
Color 1112



Color 1139



Color 273

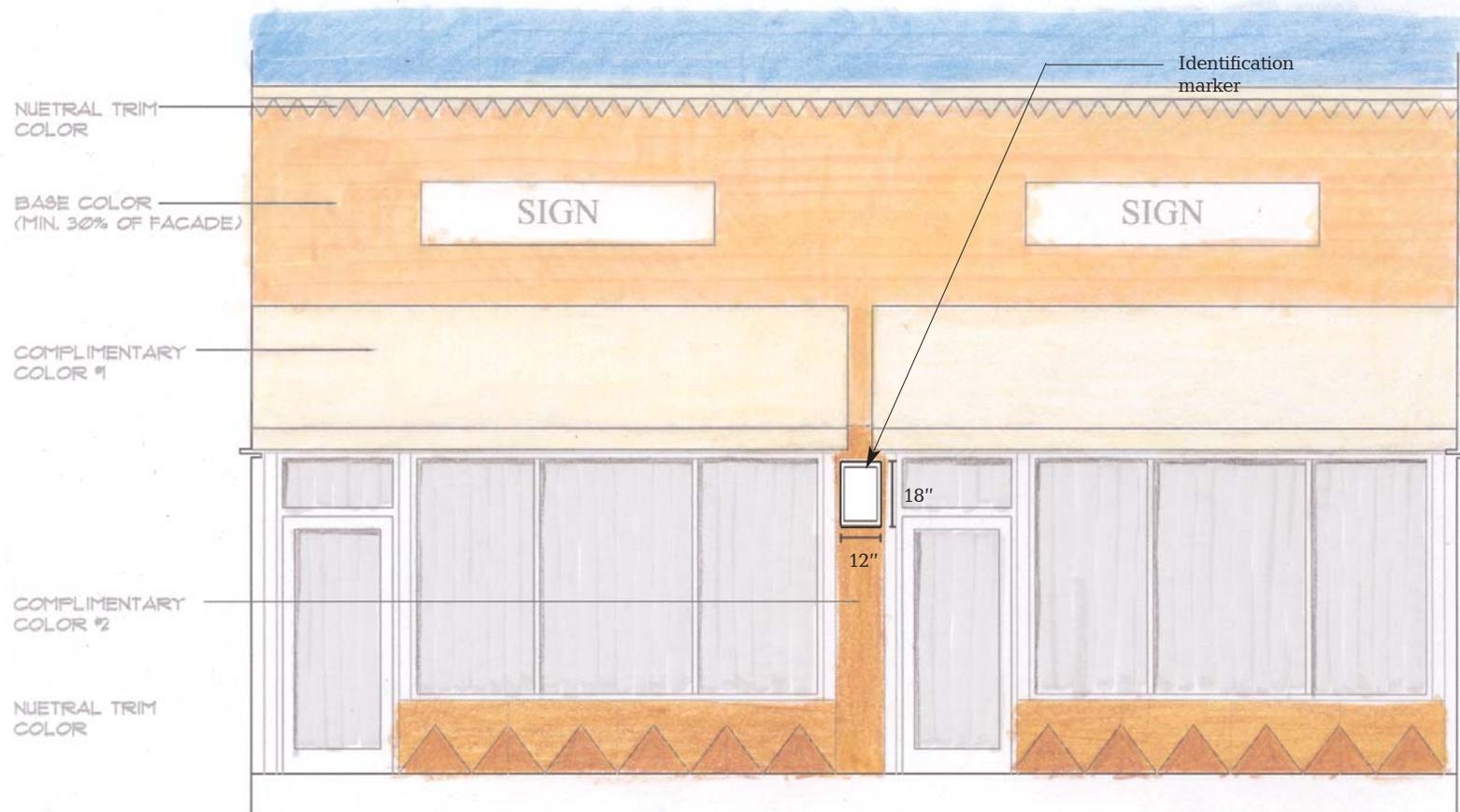


Color 151

Approved color palette base colors. See following pages for complete palette.

Note: Colors may vary and fade slightly due to different printer models and reproduction. For actual color samples, contact the City of Miami Planning Department.

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS



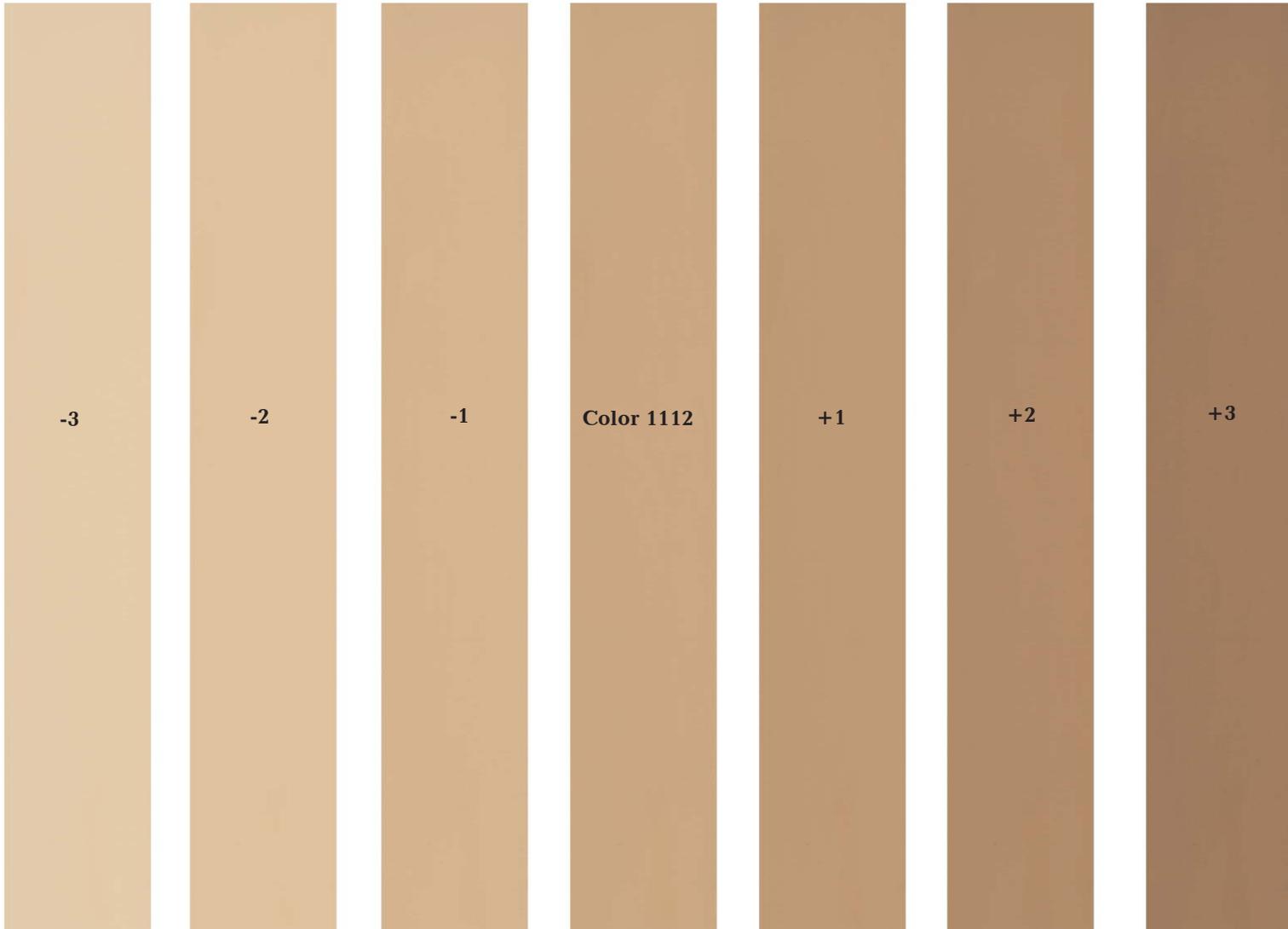
The illustration above indicates the appropriate use of color on a facade. This is only one example of one type of color combination. Other base colors and complementary colors may be used as well to enhance the color palette of the corridor.



## Facade Standards

---

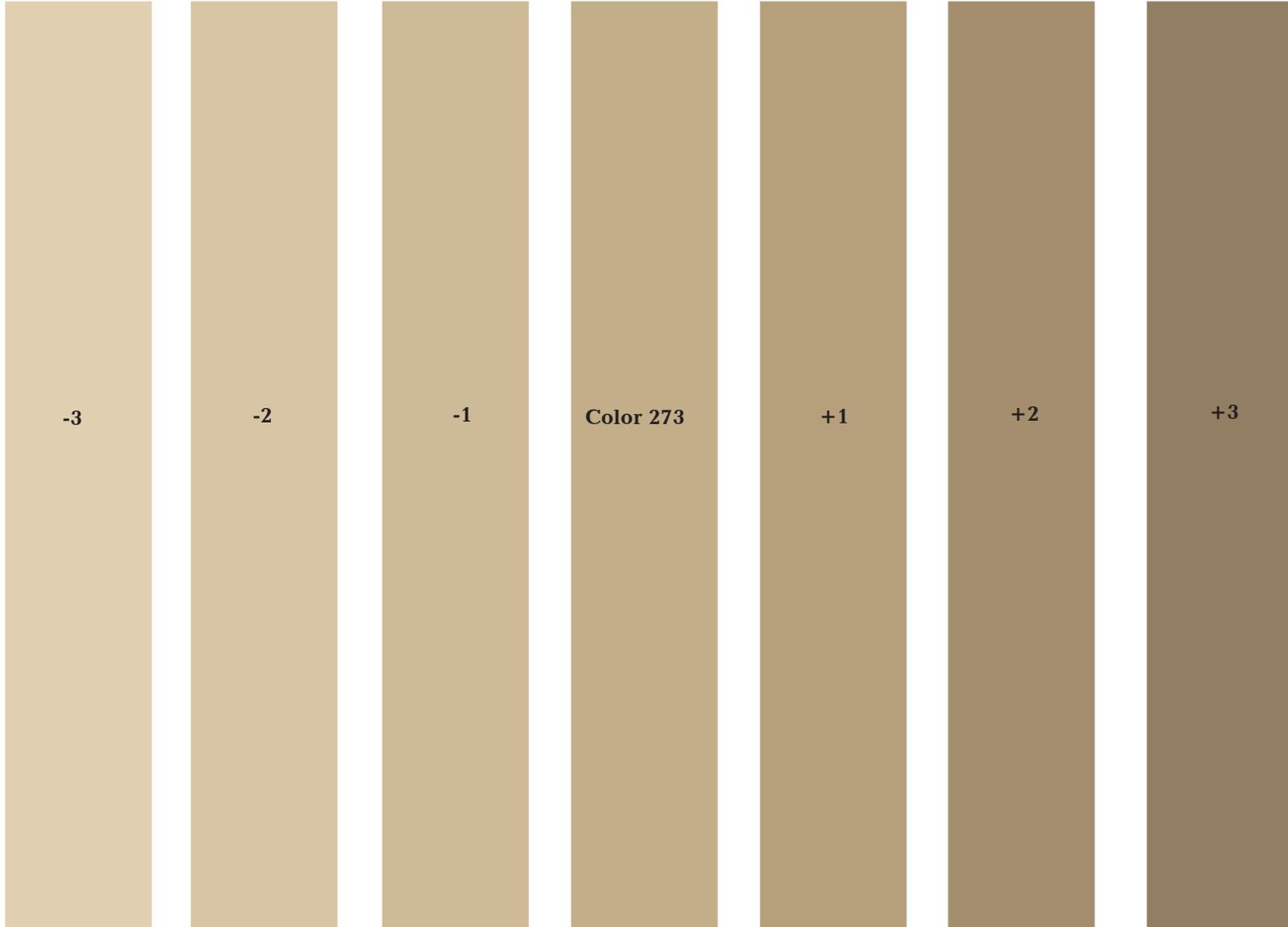
### Color



## Facade Standards

---

### Color

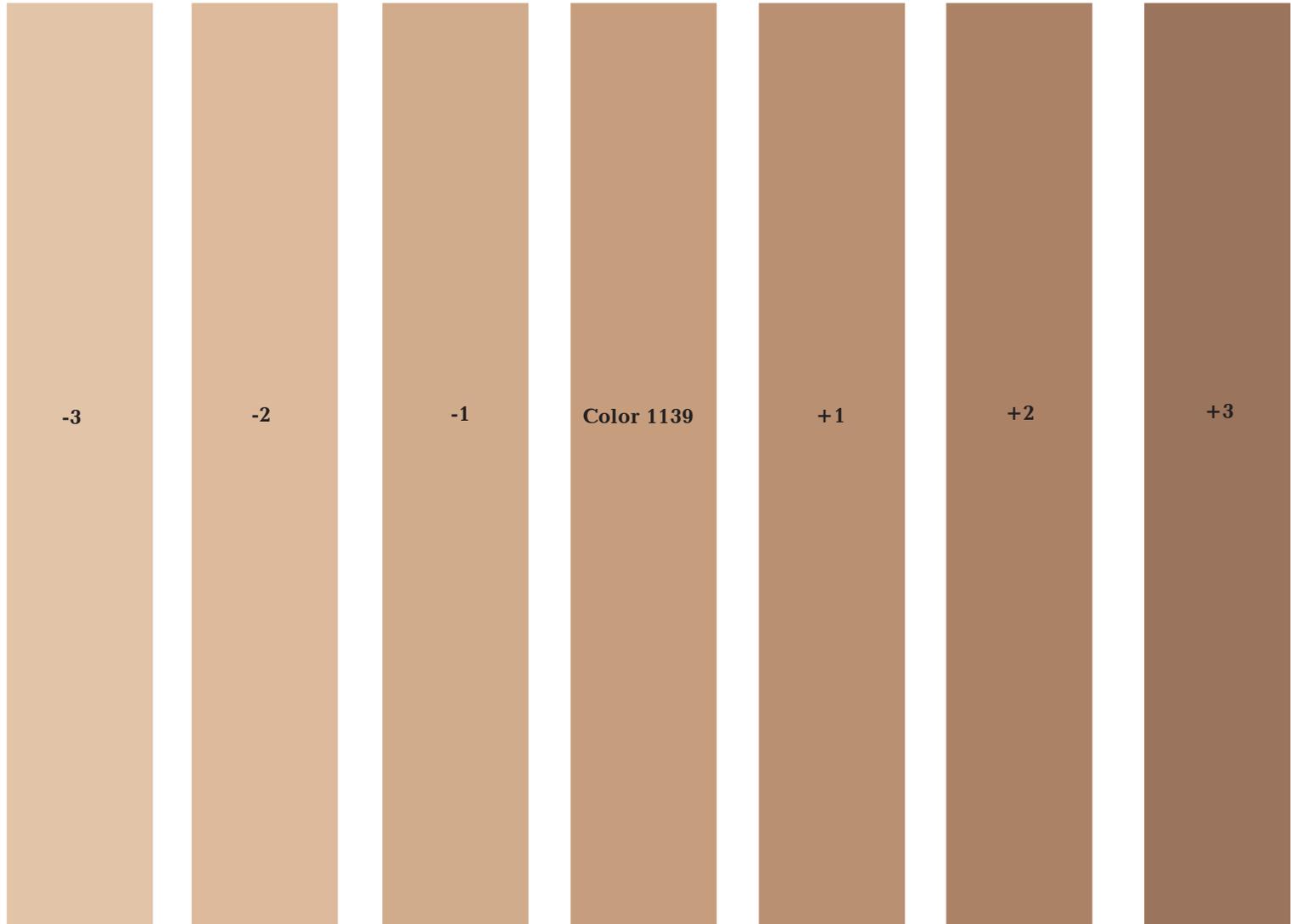




## Facade Standards

---

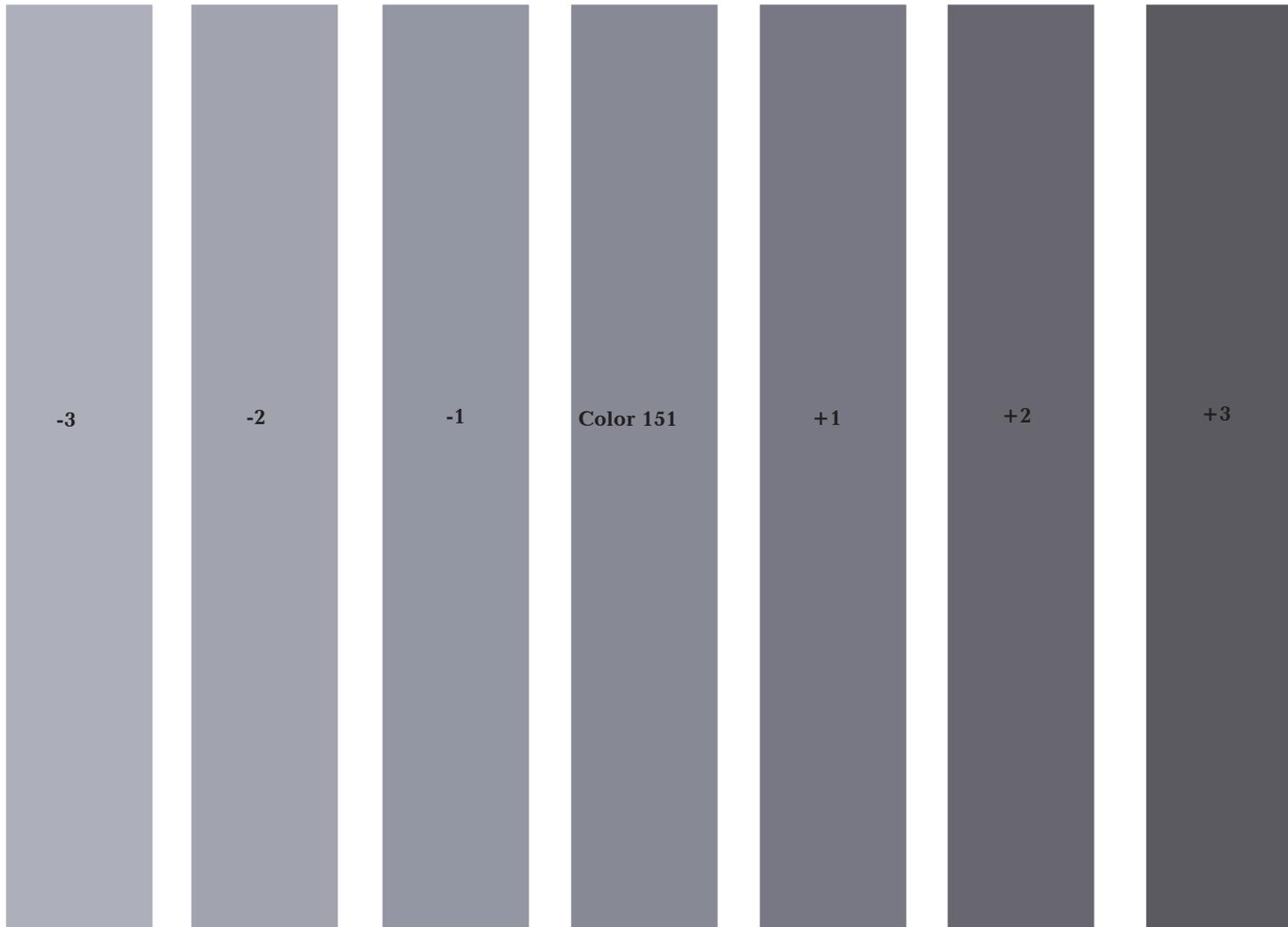
### Color



## Facade Standards

---

### Color



## Facade Standards Signage

### Background/Intent

Signage creates an image of a business before a customer or client enters. The amount, type, scale and style of the signage is significant because more signage does not necessarily bring more sales to a business. For commercial businesses, the signage needs to be minimal, to allow customers and clients to feel comfortable and not overwhelmed. It is the intent of this document to standardize the appearance and type of signage while still allowing for individuality and character unique to each business.

### Standard

**Signage type shall be consistent throughout the same building.  
Signage shall be regulated per the standards set forth herein.**

This can be accomplished using the following criteria:

- Signs of commercial establishments shall be made of materials such as wood, metal, or synthetic materials that will be more durable and meet local building codes.
- Signage may be lit in one of two ways:
  - Externally, with fixtures affixed to the building or the sign and shall wash the sign in color-corrected light, or
  - Back-lit, with the light fixtures hidden completely behind individual characters and/or icons.
- Marquis and projecting signs shall extend no more than four(4) feet from the building wall, whether horizontally or vertically oriented. Such signs shall be located within the arcade or underneath awnings. All signs shall have a minimum 7 foot clearance from the ground.

- Business signs shall be consistent for the entire building and shall not vary between storefronts. They shall be a maximum of 12" in height.
- Storefront signage shall be located in the upper and/or lower portion of the storefront glass area up to 20% combined of total glass area. Signage shall be white vinyl lettering of maximum 3" height and shall not interfere with the pedestrian's visual connection to the street.
- For lighted signs and signage-related lighting, all ballasts, connection boxes, etc. shall be hidden from pedestrian view.



This building incorporates a consistent signage design. All lettering is uniform in font, color, size, and location and is appropriately proportioned with the rest of the storefront.



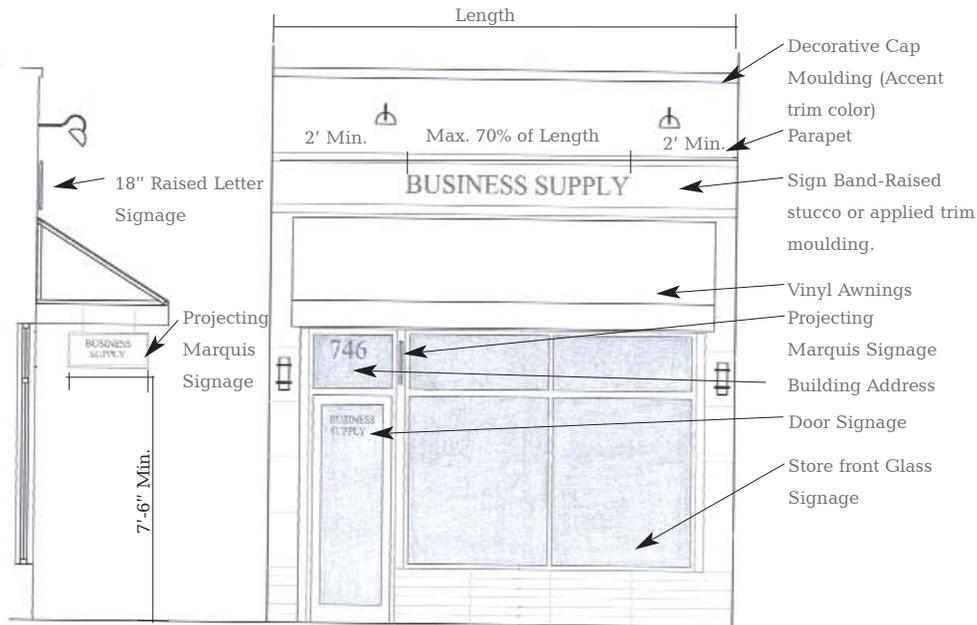
Typical spot lighting for signage.

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

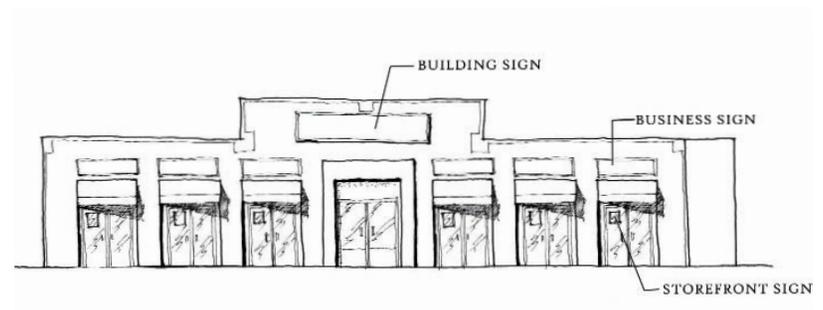
- Letters shall be made of wood, synthetic wood, plastic, or metal.
- Business signs shall be a maximum length of 70% of each individual business facade length. Such signs shall be no closer than 2 feet from the edge of the facade.
- Neon signs are permitted on the upper 1/4 of the store-front glass areas for name of the business only.
- For building-mounted signs, lettering shall be individual with no backdrop. Logos are permitted to have a backdrop.
- Building signage lettering shall be maximum 24" height. Individual letters are encouraged.
- No awning signs are permitted.



This building incorporates a consistent signage design, which is elegant, simple, and non-obtrusive.



SIGNAGE LOCATION



## Facade Standards

### Awnings

#### Background/Intent

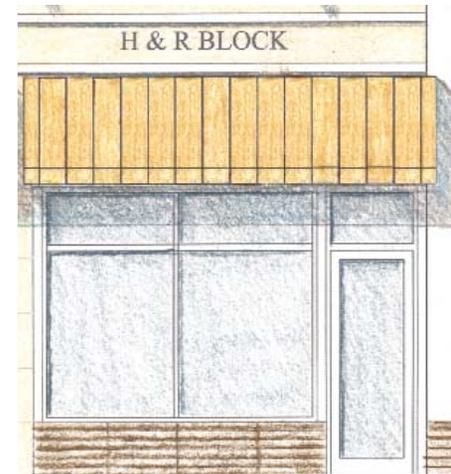
Awnings are a critical part of a facade. They provide both shade and shelter from the elements as well as affect greatly the appearance of a storefront. They also help to reinforce the pedestrian environment. Having a consistent appearance to an area is extremely important in attracting customers and “window shoppers”.

#### Standard

**Awnings shall be uniform with each other and adhere to the standards set forth herein.**

This can be accomplished using the following criteria:

- Awnings shall be only made from traditional, durable materials, such as vinyl and shall be fire resistant. Metal is not permitted.
- Weblom or any other approved manufacturers shall be used.
- Awnings can be used for commercial storefronts, but they must be only used for functionality – i.e. providing shade, weather protection, etc.
- Awnings shall be allowed to encroach up to three (3) feet from of the face of curb.
- Awnings may have side panels, but no panels to enclose the underside of the awning.
- The awnings must be triangular, curved, or straight in section (refer to sketches, next page).
- All awnings on a single building shall be the same material and color. Minor variations in size are acceptable to accommodate building openings.
- Awning colors shall complement the building color palette. Refer to manufacturer’s color selections.



It is possible to combine multiple colors on awnings as long as the colors are from the pre-approved palette and are complementary to each other.



This awning complements the facade well by not taking the focus away from the building, but rather blending in with the colors and the scale. The lettering on the awning is likewise well-sized and appropriately placed on the vertical flap.



## Facade Standards

### Arcades

#### Background/Intent

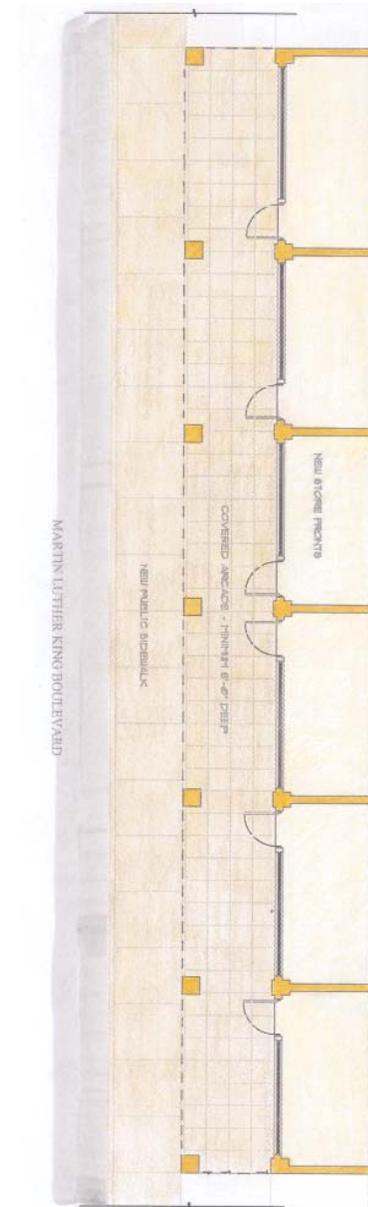
Arcades not only provide essential shade for pedestrians, but they are also powerful elements that can greatly influence the appearance of a building's facade and its relationship to the street.

It is the intent of these standards to influence designers to properly use these elements to enhance the connectivity of the building language throughout the corridor as well as provide continuous opportunity for shade.

#### Standard

**Arcades may encroach into the setback area up to the right-of-way to provide a continuous canopy for pedestrian use.**

- Arcade clear width (excluding columns) shall be a minimum of 8'. Proportions shall be taller than wide.
- Minimum span between columns shall be 8'.
- Arcade must span the entire length of the building frontage.



Typical Arcade

## Facade Standards

### Storefront Security

#### Background/Intent

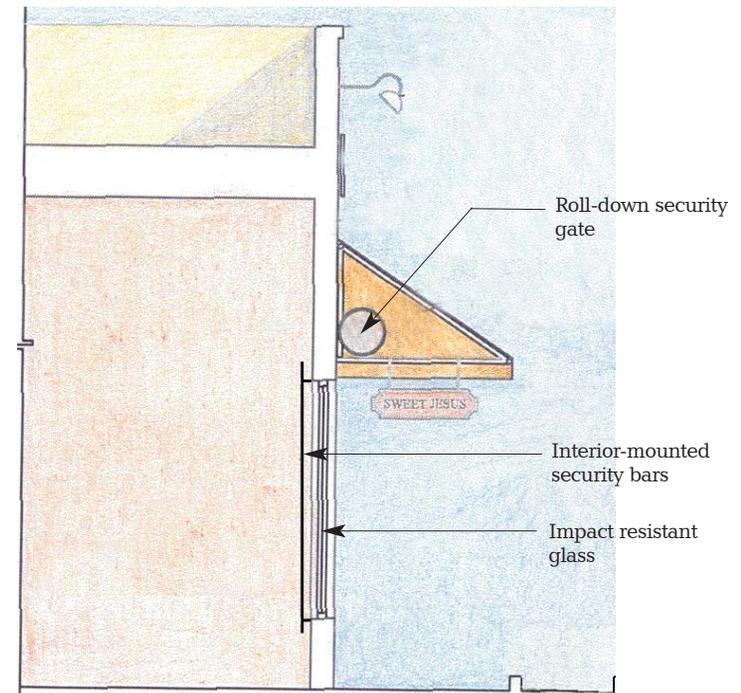
While storefront security is extremely important to business owners, a view of unsightly and unattractive security devices can have an adverse affect on the profitability of the store by making it seem uninviting to customers as well as potential perpetrators.

These standards are intended to provide a balance between proper store security and a higher degree of storefront visibility by implementing visually unobtrusive security devices.

#### Standard

**Storefront security shall be made up of one of the following devices:**

- 1. Impact resistant glass (preferred option)
- 2. Visually permeable security wall
- 3. Solid roll-down security gates
- 4. Security bars - must be on the inside of windows and painted black.
- All security gates shall roll down from top of building.
- Accordion security bars are permitted on the inside of the building only.



Viable Building Security Options

## Facade Standards

### Lighting

#### Background/Intent

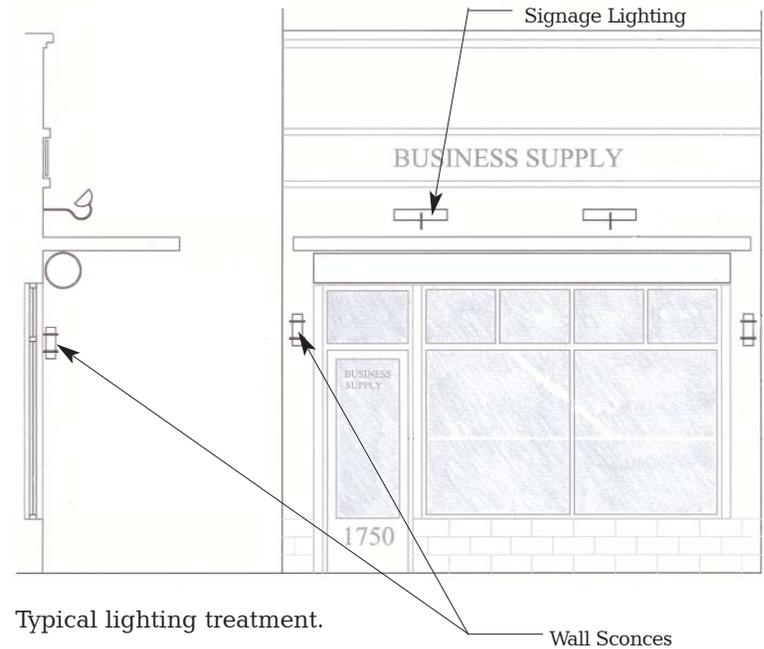
Lighting enhances the overall quality of the facade by highlighting certain aspects, such as architectural features and signage. Proper facade lighting can contribute to the success of the business by creating a perception of safety and interest at night. The intent of this document is to provide for the proper placement and use of facade lighting to maximize the potential of each property.

#### Standard

**Building facades and areas in front of the building shall be sufficiently lit to clearly illuminate the signage and facade as well as to provide a minimum of one(1) footcandle along exterior private areas.**

This can be accomplished using the following criteria:

- All lighting shall be metal halide or other white light sources (refer to UES standards for proper footcandle requirements for various spaces).
- All lighting elements, including site lighting, shall be of the same palette and shall relate in design to the building.
- Storefront lighting shall illuminate all signs, entrances, displays, and interiors at appropriate levels.
- Channel lettering signs may be backlit with incandescent spotlights or fluorescent or neon tubing.
- Incandescent spotlights may be placed at intervals above channel lettering signs.
- Architectural features such as decorative cornices, columns, openings, balconies, and other elements shall be accentuated for multi-story buildings.
- All lighting for the first 15' of the facade must be wall-mounted. No lighting shall be mounted on awnings. For continuous eyebrow, mount on top only.



Typical lighting treatment.

## Continuous Curb-Cuts



The image above shows a continuous curb-cut, which is not allowed under these guidelines. This means that a driveway shall follow the dimension requirements and not allow for a break in curbing that extends further than the allowable driveway width.

## Facade Standards

### Parking

#### Background/Intent

Parking is a necessity for the success of most properties. The intent of these standards is to provide a visually aesthetic screen which separates parking areas from the corridor to enhance the overall character of the boulevard.

#### Standard

**Parking shall be placed out of view and allow for minimum visual impact to the area.**

This can be accomplished using the following criteria:

- All new parking shall be located on the side or rear of the property.
- Off-street parking shall be screened on both the side and rear with a non-transparent material, such as stucco finish wall, decorative or iron picket fence, with landscape hedge or wall covering the bottom 30 inches to a minimum height of 6'. No chain-link fences shall be allowed.
- A minimum landscape buffer shall be placed between visual screen and back of sidewalk. Landscape material shall be a continuous hedge with a minimum height of 3' at planting.
- Where parking areas abut side or rear streets, landscape shall be planted to eliminate any visibility into the parking area by pedestrians or drivers. Planting shall be 3' at the time of planting.
- Entrances to commercial properties along major thoroughfares shall be through side streets. If this is determined to not be feasible by staff, an administrative waiver may be granted.
- Continuous curb cuts shall not be permitted.



The sketch above illustrates a six foot high decorative iron picket fence and continuous landscape hedge shielding an unsightly parking area (see image below) from the public's view.



The image above shows how screening is needed to shield a parking area from public view.

## Facade Standards

---

### Garbage Disposal

#### Background/Intent

As with any urban corridor, garbage disposal becomes an issue of concern as it applies to the aesthetics of the streetscape. Proper screening of dumpsters and trash cans should be implemented to keep the service-related elements of a building out of the public's site.

#### Standard

**Dumpsters, trash cans and other garbage or back-of-house related uses shall be screened from the public right of way by a solid/opaque screen or gate at least 6' in height.**

This can be accomplished using the following criteria:

- Screens may be of the following materials, providing that they are 100% opaque:
  - metal, iron, aluminum, wood(must be painted)
- Garbage disposal areas shall not be located along the front facades of any property.
- Maximum width for a dumpster and gate fronting a right of way shall be six(6) feet. Such gates must be of a decorative style matching that of the building and remain closed at all times aside from the removal of dumpsters and/or trash cans.



The dumpster and other utilities above are appropriately hidden by a solid gate.

## Facade Standards

### Mechanical and Electrical Equipment

#### Background/Intent

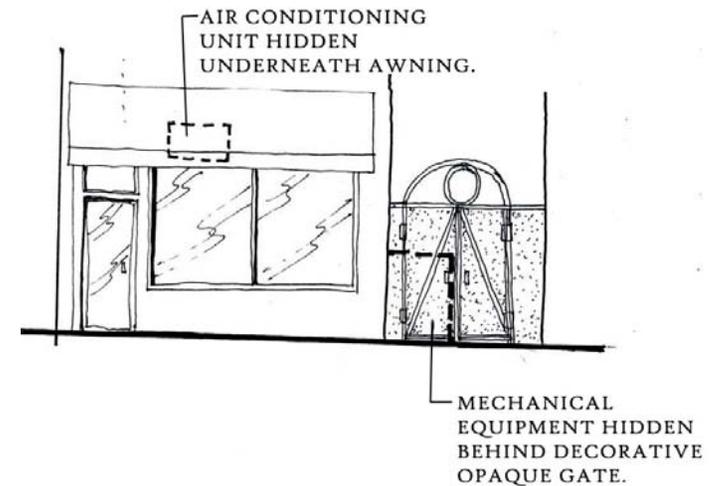
Another essential part of a functional urban building is the various mechanical components. These elements need to be included within the overall aesthetic improvements of the facade since many of these components currently exist in plain sight of the right of way. It is important to incorporate other facade elements such as awnings to shield these elements from the public view.

#### Standard

**All mechanical equipment including air conditioning units, meters, and switch panels shall be completely screened from the public right of way by a solid/opaque screen or gate at least 6' in height. If mounted on the facade, they shall be screened by awning or decorative enclosure.**

This can be accomplished using the following criteria:

- Screens may be of the following materials providing they are 100% opaque.
  - metal, decorative wrought iron, aluminum, wood(must be painted)
- Mechanical equipment shall not be displayed along the right of way frontage.
- Wall mounted air conditioning units shall be screened by an awning.
- Maximum width for a mechanical equipment gate fronting a right of way shall be six(6) feet. Such gate must be of a decorative style matching that of the building and remain closed at all times

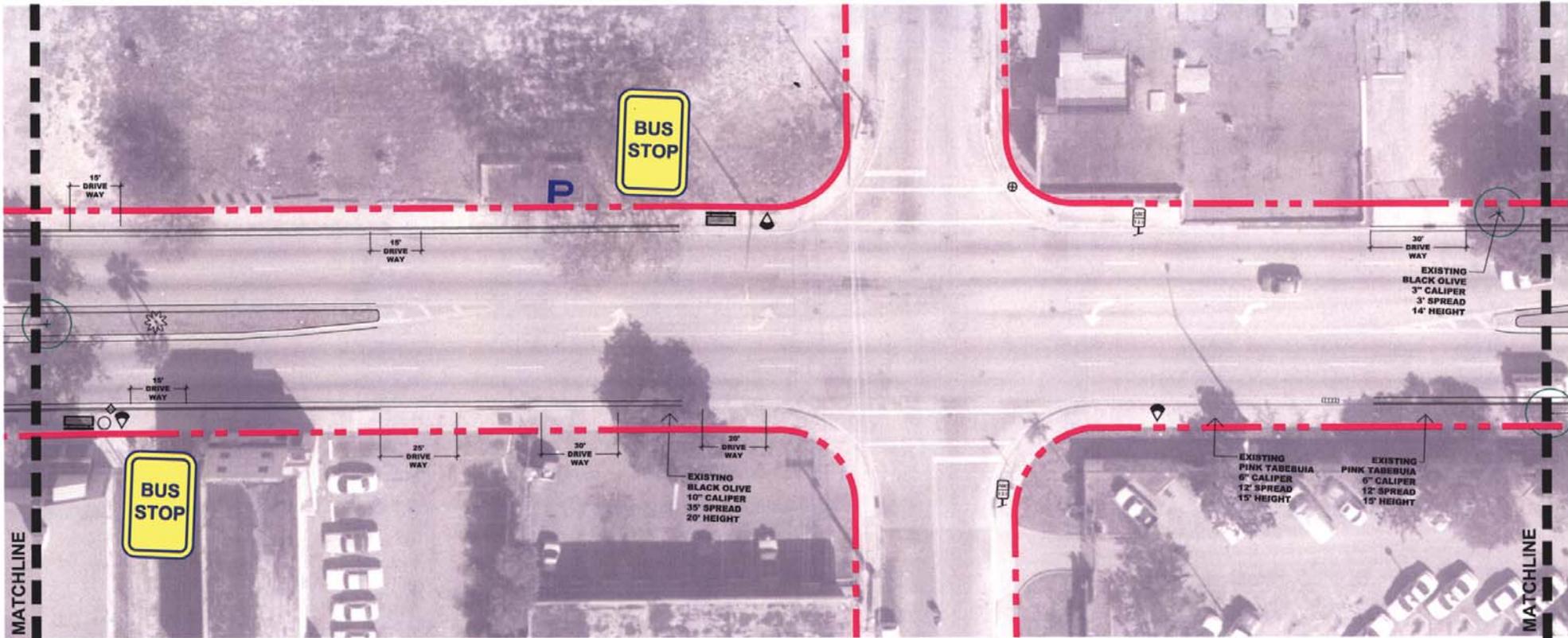


The equipment in the image above is screened completely by an awning and/or a gate.

**A P P E N D I X**

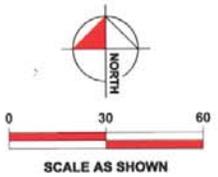


# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS



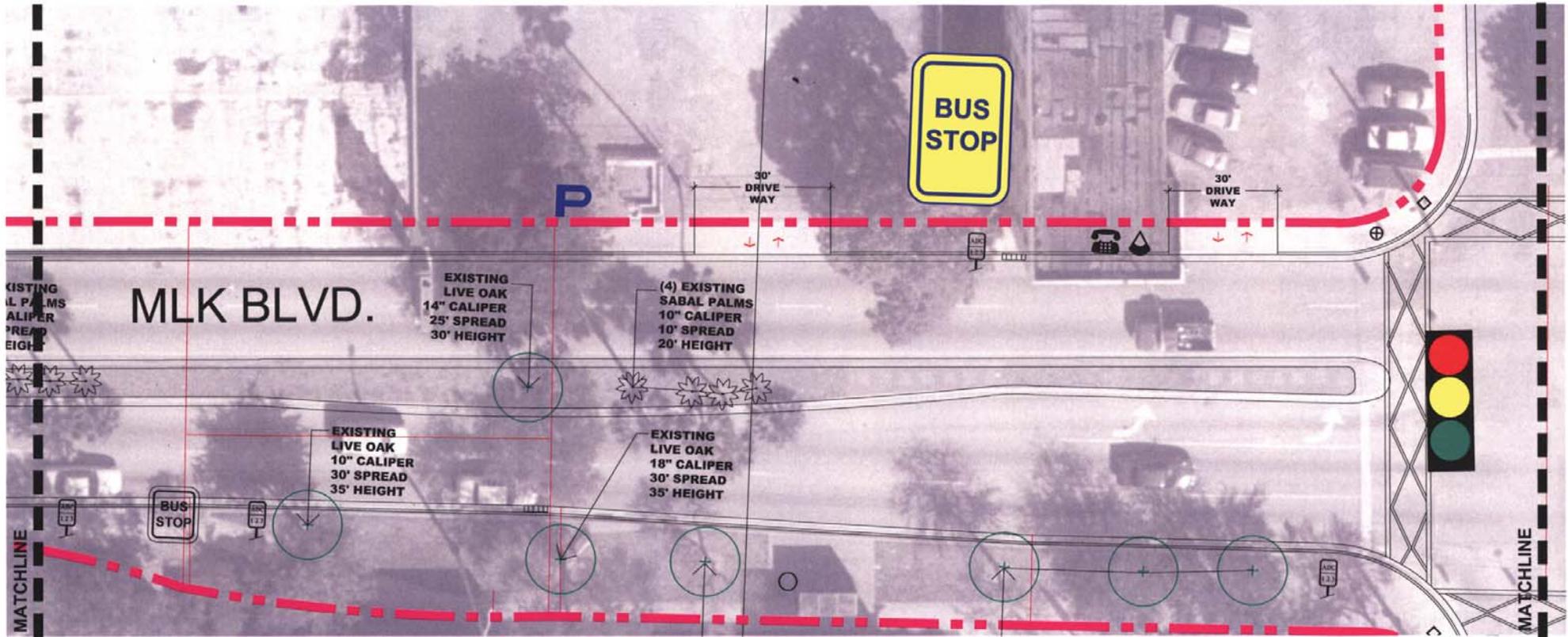
**LEGEND:**

- RIGHT OF WAY
- EXISTING EDGE OF PAVEMENT
- EXISTING BUS STOP
- EXISTING SIGNALIZED INTERSECTION
- P LINEAR PARK LOCATION
- EXISTING AND PROPOSED DRIVEWAYS
- + EXISTING CANOPY TREE
- \* EXISTING PALMS
- EXISTING BENCH
- EXISTING LIGHT POLE
- ☎ EXISTING PUBLIC PHONE
- T EXISTING SIGN
- ⊕ EXISTING FIRE HYDRANT
- ⌌ EXISTING STORM DRAIN
- EXISTING GARBAGE BIN
- ◇ EXISTING UTILITY POLE
- EXISTING ABOVE GROUND UTILITY BOX

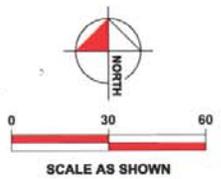




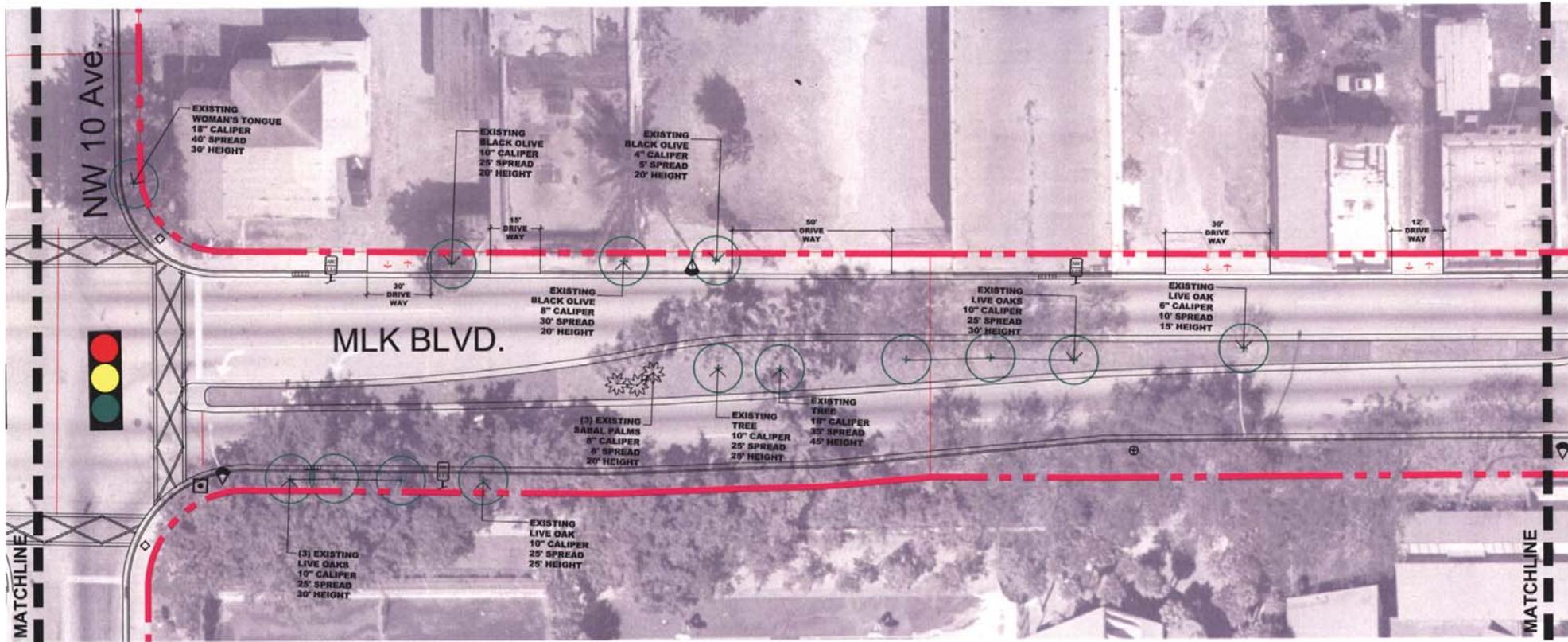
# DR. MARTIN LUTHER KING, JR. BOULEVARD



- LEGEND:**
- RIGHT OF WAY
  - EXISTING EDGE OF PAVEMENT
  - EXISTING BUS STOP
  - EXISTING SIGNALIZED INTERSECTION
  - P** LINEAR PARK LOCATION
  - EXISTING AND PROPOSED DRIVEWAYS
  - EXISTING CANOPY TREE
  - EXISTING PALMS
  - EXISTING BENCH
  - EXISTING LIGHT POLE
  - EXISTING PUBLIC PHONE
  - EXISTING SIGN
  - EXISTING FIRE HYDRANT
  - EXISTING STORM DRAIN
  - EXISTING GARBAGE BIN
  - EXISTING UTILITY POLE
  - EXISTING ABOVE GROUND UTILITY BOX

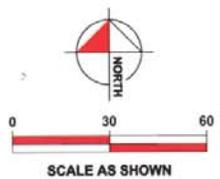


# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS



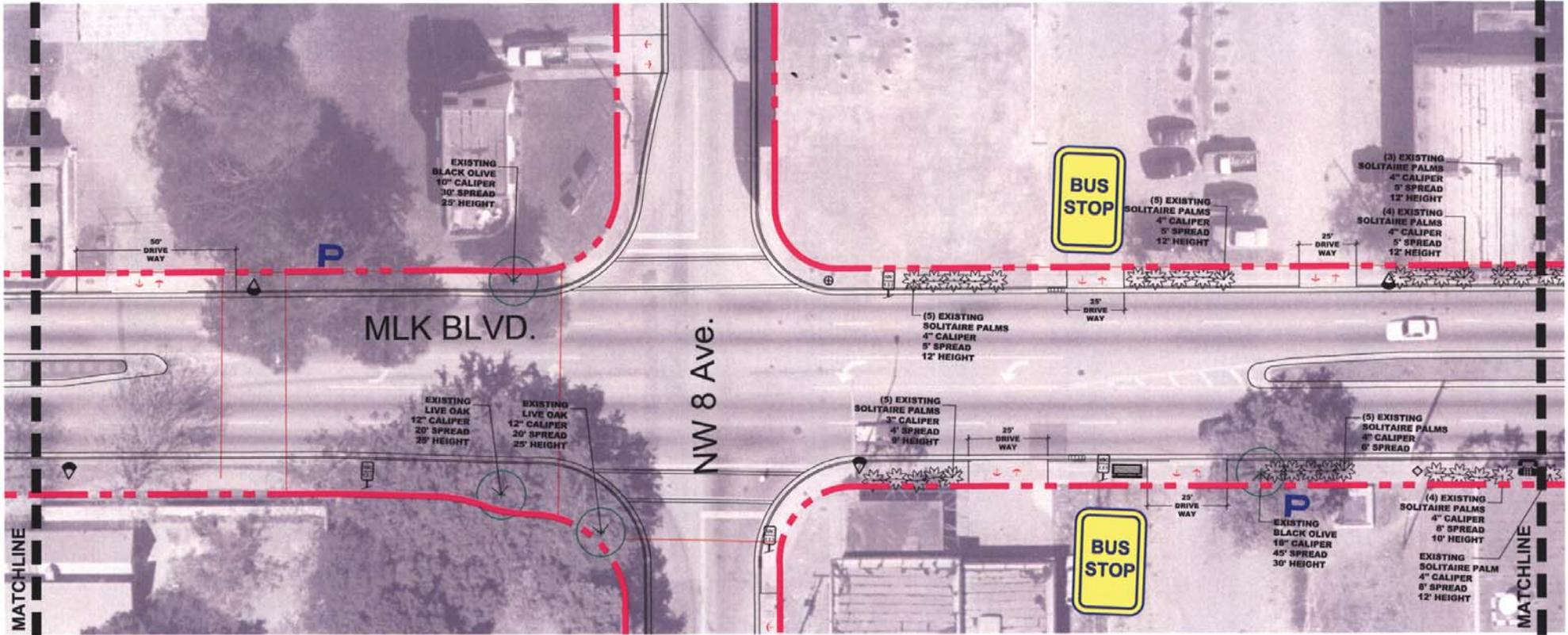
**LEGEND:**

- RIGHT OF WAY
- EXISTING EDGE OF PAVEMENT
- EXISTING BUS STOP
- EXISTING SIGNALIZED INTERSECTION
- LINEAR PARK LOCATION
- EXISTING AND PROPOSED DRIVEWAYS
- EXISTING CANOPY TREE
- EXISTING PALMS
- EXISTING BENCH
- EXISTING LIGHT POLE
- EXISTING PUBLIC PHONE
- EXISTING SIGN
- EXISTING FIRE HYDRANT
- EXISTING STORM DRAIN
- EXISTING GARBAGE BIN
- EXISTING UTILITY POLE
- EXISTING ABOVE GROUND UTILITY BOX





# DR. MARTIN LUTHER KING, JR. BOULEVARD

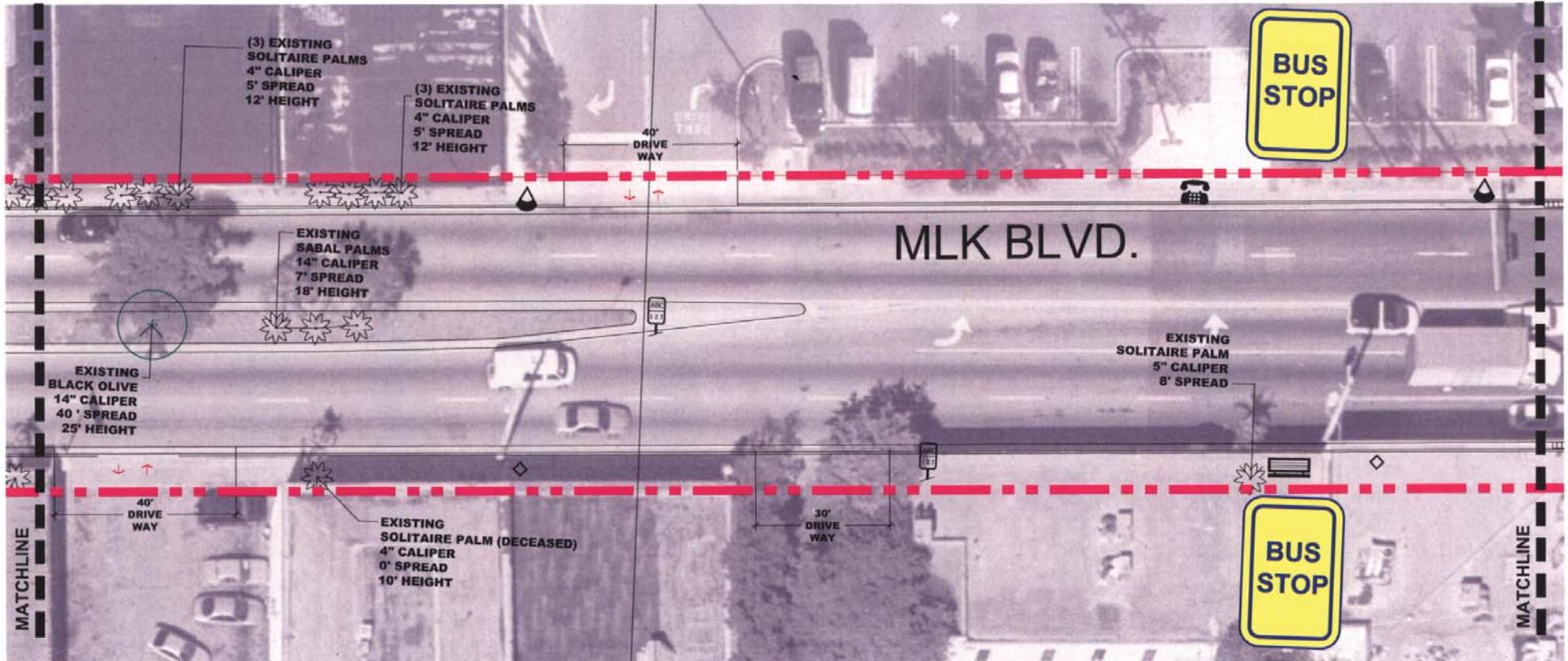


**LEGEND:**

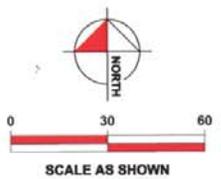
- RIGHT OF WAY
- EXISTING EDGE OF PAVEMENT
- EXISTING BUS STOP
- EXISTING SIGNALIZED INTERSECTION
- P LINEAR PARK LOCATION
- + + EXISTING AND PROPOSED DRIVEWAYS
- EXISTING CANOPY TREE
- EXISTING PALMS
- EXISTING BENCH
- EXISTING LIGHT POLE
- EXISTING PUBLIC PHONE
- EXISTING SIGN
- EXISTING FIRE HYDRANT
- EXISTING STORM DRAIN
- EXISTING GARBAGE BIN
- EXISTING UTILITY POLE
- EXISTING ABOVE GROUND UTILITY BOX

SCALE AS SHOWN

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

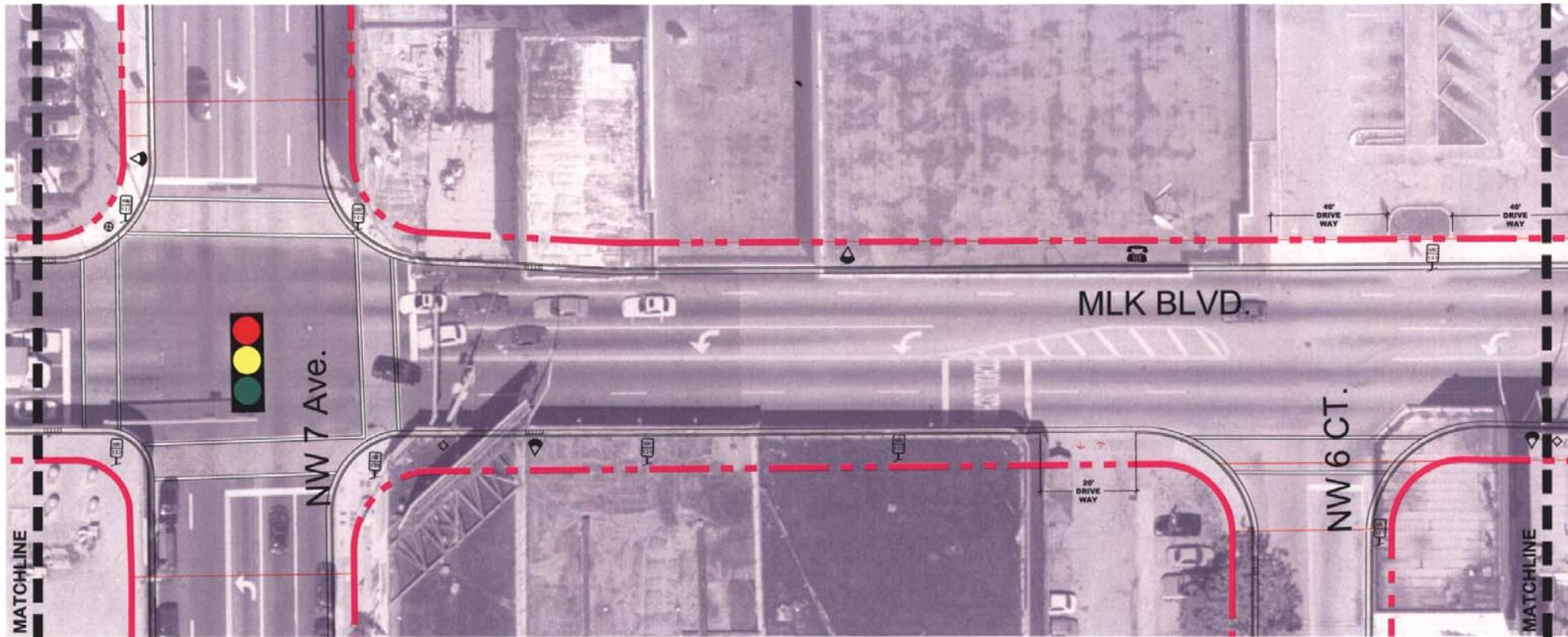


- LEGEND:**
- RIGHT OF WAY
  - EXISTING EDGE OF PAVEMENT
  - EXISTING BUS STOP
  - EXISTING SIGNALIZED INTERSECTION
  - LINEAR PARK LOCATION
  - ↕ EXISTING AND PROPOSED DRIVEWAYS
  - EXISTING CANOPY TREE
  - EXISTING PALMS
  - EXISTING BENCH
  - EXISTING LIGHT POLE
  - EXISTING PUBLIC PHONE
  - EXISTING SIGN
  - EXISTING FIRE HYDRANT
  - EXISTING STORM DRAIN
  - EXISTING GARBAGE BIN
  - EXISTING UTILITY POLE
  - EXISTING ABOVE GROUND UTILITY BOX

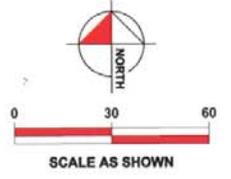




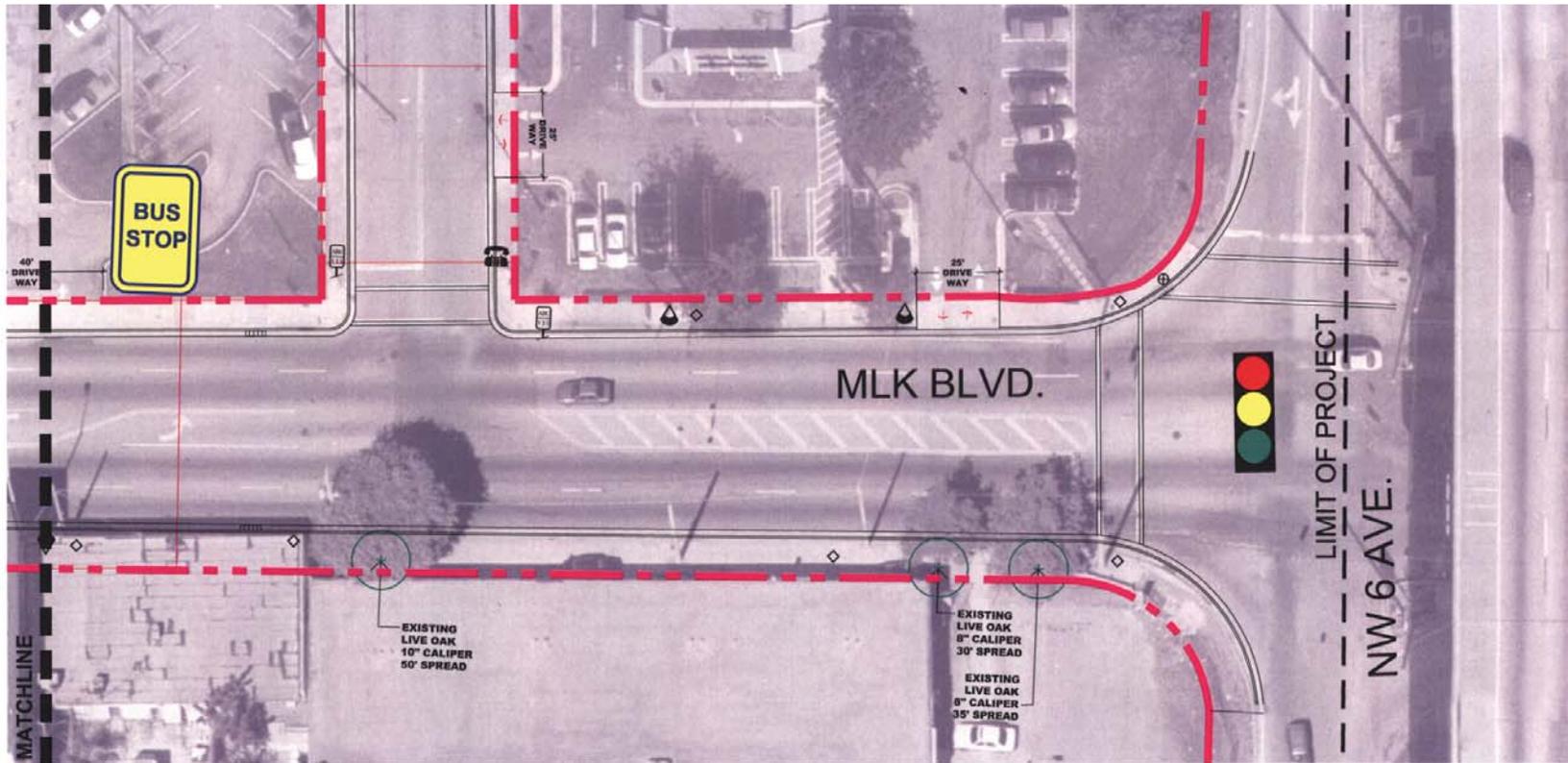
# DR. MARTIN LUTHER KING, JR. BOULEVARD



- LEGEND:**
- RIGHT OF WAY
  - EXISTING EDGE OF PAVEMENT
  - BUS STOP EXISTING BUS STOP
  - EXISTING SIGNALIZED INTERSECTION
  - P LINEAR PARK LOCATION
  - + + EXISTING AND PROPOSED DRIVEWAYS
  - + EXISTING CANOPY TREE
  - \* EXISTING PALMS
  - EXISTING BENCH
  - EXISTING LIGHT POLE
  - EXISTING PUBLIC PHONE
  - EXISTING SIGN
  - EXISTING FIRE HYDRANT
  - EXISTING STORM DRAIN
  - EXISTING GARBAGE BIN
  - EXISTING UTILITY POLE
  - EXISTING ABOVE GROUND UTILITY BOX

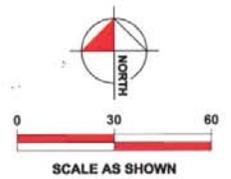


# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS



**LEGEND:**

- RIGHT OF WAY
- EXISTING EDGE OF PAVEMENT
- EXISTING BUS STOP
- EXISTING SIGNALIZED INTERSECTION
- LINEAR PARK LOCATION
- EXISTING AND PROPOSED DRIVEWAYS
- EXISTING CANOPY TREE
- EXISTING PALMS
- EXISTING BENCH
- EXISTING LIGHT POLE
- EXISTING PUBLIC PHONE
- EXISTING SIGN
- EXISTING FIRE HYDRANT
- EXISTING STORM DRAIN
- EXISTING CARBAGE BIN
- EXISTING UTILITY POLE
- EXISTING ABOVE GROUND UTILITY BOX





## HISTORICAL DESIGNATION REPORTS

NUMBER 1 PALLBEARERS  
ASSOCIATION OF AMERICA  
BUILDING  
801 NW 62<sup>nd</sup> Street

---

REPORT OF THE CITY OF MIAMI  
PRESERVATION OFFICER  
TO THE HISTORIC AND ENVIRONMENTAL PRESERVATION BOARD  
ON THE POTENTIAL DESIGNATION OF THE  
NUMBER 1 PALLBEARERS ASSOCIATION OF AMERICA BUILDING  
AS A HISTORIC SITE

Designation Report



City of Miami

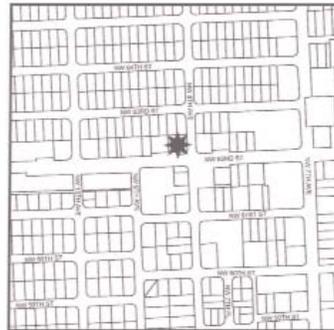
Prepared by Ellen Uguccioni for Janus Research, Consultant

Prepared by Sarah F. Eaton, Preservation Officer

Passed and Adopted on \_\_\_\_\_

Resolution No. \_\_\_\_\_

NUMBER 1 PALLBEARERS ASSOCIATION  
OF AMERICA BUILDING  
801 NW 62nd Street



⊕  
location



⊕  
site plan

TABLE OF CONTENTS

I. General Information.....	5
II. Significance .....	6
III. Description .....	7
IV. Planning Context .....	9
V. Bibliography .....	10



# DR. MARTIN LUTHER KING, JR. BOULEVARD

## I. GENERAL INFORMATION

### Historic Names:

None

### Current Name:

Number 1 Pallbearers Association of America Building<sup>1</sup>

### Location:

801 NW 62<sup>nd</sup> Street

### Present Owner:

Number 1 Pallbearers Association of America  
4716 NW 16<sup>th</sup> Avenue  
Miami, Florida 33142-4127

### Present Use:

Vacant

### Zoning District:

6300 Highly Restricted Retail/ Office Building

### Tax Folio Number

01-3114-036-0350

### Boundary Description

Lot 23 less the south 15 feet and all Lot 24, Block 2, Seventh Avenue Park Subdivision as recorded in Plat Book 17- 17 of the Public Records of Miami-Dade County, Florida.

### Classification:

Historic Site

<sup>1</sup> The word "pallbearer" means someone who attends to a coffin at a funeral. The correct form of the word has no separation between "pall" and "bearer." The assessor's records and other references have chosen to separate the words. In the interest of accuracy, this report will use the correct version throughout.

## II. SIGNIFICANCE

Specific Dates: 1947

Architect: Unknown

Builder/  
Contractor: Unknown

### Statement of Significance

Northwest Miami has historically been an area that has seen a demographic shift from once all white communities to predominantly African-American neighborhoods. While this area had been sporadically settled as a farming community in the early 20<sup>th</sup> Century, the greatest round of home building occurred in the 1920s, designed as moderately priced housing for the white middle-class. The area known as Liberty City (an area city planners now call "Model City") was community that witnessed this demographic shift.

The influx of the African-American population to Liberty City began in earnest during the 1940s, following the construction of the Liberty Square Housing Project, which opened in the late 1930s. Liberty City provided relief to the extremely crowded conditions of Overtown, where most of Miami's African-Americans had settled at the beginning of the 20<sup>th</sup> century.

The International Association of Pallbearers was created during an era of segregation, and is a dues-paying society. Their mission is to provide financial support for members and assistance to other black families who could not afford medical care or a funeral (Robinson, *Miami Herald*). At its height, the Pallbearers Association had 18 lodges with hundreds of members across Florida, including six in Miami, and at least one in Hallandale. Although their membership is dwindling, the organization is still in existence.

In an interview with a *Miami Herald* journalist, Wittington Johnson, a retired University of Miami professor and Overtown native, explained that the Pallbearers and similar groups were a precursor to today's insurance companies. They filled the needs of African-Americans, who because of racist practices were refused burial insurance. Membership was through an application process, and was open to anyone who did not have a life-threatening illness when they applied (Robinson, *Miami Herald*.)

The Miami Chapter of the International Association of Pallbearers met in a Masonic lodge house in Overtown until they were displaced by the construction of Interstate 95 in the early 1960s. They moved to the building at 801 NW 62<sup>nd</sup>

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

Street in Liberty City, where they still meet. Today, there are fewer numbers of members in the Association, and many of them are elderly. Still, when a dues paying member dies, the group sends a \$300 donation to the survivors.

### III. DESCRIPTION

The building faces south onto NW 62<sup>nd</sup> Street and is located at the northwest corner of NW 62<sup>nd</sup> Street and NW 8<sup>th</sup> Avenue. The tax card indicates that the building was designed for both commercial and residential uses. The commercial aspect of the building was limited to the first floor, while the second story provided living quarters that could be accessed by an exterior staircase on the north side of the building. The one story section is particularly interesting as its semicircular shape puts the entrance at a diagonal with the street.

The building, constructed in 1947, is a late example of the Art Moderne style of architecture. The Art Moderne style, an offshoot of the Art Deco style, uses its geometry for effect. In this example the lines of the one story, flat-roofed building block are curved. The doorway becomes the principal focal point of the building, as it is set into the center of this curved elevation; is partially recessed, and is flanked by vertical score lines simulating the appearance of pilasters. An eyebrow roof projects above the door and side bays further emphasizing the building's sense of movement. Fenestration consists of a rectangular plate glass window (now boarded) on the westernmost side, and a circular window on the easternmost side (now blocked).

The second story section of the building occupies approximately two-thirds of the building's whole. This section is rectangular and terminates in a flat roof. Eyebrow ledges extend out above the windows of this section. Although the windows have been replaced with aluminum awning windows, and other apertures are blocked entirely, the building continues to maintain its integrity, and is an unusual design and mixed-use type for this area of the city.

#### Relationship to Criteria for Designation

The Number 1 Pallbearers Association of America Building has significance in the historical and architectural heritage of the City of Miami; possesses integrity of design, setting, materials, workmanship, feeling and association; and is eligible for designation under the following criteria:

3. Exemplifies the historical, cultural, political, economic, or social trends of the community.

The Number 1 Pallbearers Association of America Building at 801 NW 62<sup>nd</sup> Street represents a unique resource in this predominantly African-American neighborhood. The Pallbearers Association created a program that supported the African-American community during a time when exclusionary racist policies excluded them from obtaining burial insurance. In a larger sense, the Pallbearers

Association, which often met within churches, was a source of genuine compassion and a rallying force for the citizens of the community.

5. Embodies those distinguishing characteristics of an architectural style, or period, or method of construction.

The Number 1 Pallbearers Association of America Building is a distinctively designed Art Moderne building that is unusual in this part of the city. Even though some of the windows have been changed, or apertures boarded, this building continues to maintain integrity and convey its unique character.



# DR. MARTIN LUTHER KING, JR. BOULEVARD

## IV. PLANNING CONTEXT

### Present Trends and Conditions:

The building at 820 NW 62<sup>nd</sup> Street is a particularly well designed building that occupies a prominent corner in Liberty City. With the revitalization effort that has been proposed along Martin Luther King Boulevard, this building is a particularly distinctive historic building that can become a defining landmark in the neighborhood.

### Preservation Incentives:

The current zoning allows for a commercial use of the property. If the building were rehabilitated and improvements made that would increase its assessed value, the owner would be eligible for an ad valorem tax incentive through Miami-Dade County. Further, if designated by the Miami Historic and Environmental Review Board, the building will be protected for the future and continue to occupy an important corner in the neighborhood.

## V. BIBLIOGRAPHY

Dunn, Marvin. *Black Miami in the Twentieth Century*. (Gainesville: University Press of Florida) 1997.

George, Paul S. and Thomas K. Peterson. "Liberty Square: 1933-1987, The Origins and Evolution of a Public Housing Project. *Tequesta, The Journal of the Historical Association of Southern Florida*, Volume 48, 1988.

Robinson, Andrea. "Lights Dimming on Black Self-Help Group" *The Miami Herald*, 22 April 2004, p. 1A.

R.L. Polk's City Directories, 1953, 1956, 1957, 1962, 1966, 1967, 1971, 1974.

Whiffen, Marcus. *American Architecture since 1780: A Guide to the Styles* Fifth Printing (Cambridge, Massachusetts: MIT Press) 1999.

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

ILLUSTRATIONS: Number 1 Pallbearers Association of America  
801 NW 62<sup>nd</sup> Street



South elevation on NW 8<sup>th</sup> Avenue  
View Looking West  
Janus Research, December 2004



West Elevation  
View looking northwest  
Janus Research, December 2004



# DR. MARTIN LUTHER KING, JR. BOULEVARD



East elevation on NW 8<sup>th</sup> Avenue  
View looking west  
Janus Research, December 2004



Corner Entrance  
South and east elevations  
View looking northwest  
Janus Research, December 2004

---

## LABORERS UNION LOCAL 478

799 NW 62<sup>nd</sup> Street

---

### Designation Report



View looking northeast



City of Miami

REPORT OF THE CITY OF MIAMI  
PRESERVATION OFFICER  
TO THE HISTORIC AND ENVIRONMENTAL PRESERVATION BOARD  
ON THE POTENTIAL DESIGNATION OF THE  
LABORERS UNION LOCAL 478  
AS A HISTORIC SITE

Prepared by Ellen Ugucioni for Janus Research, Consultant

Prepared by Sarah E. Eaton, Preservation Officer

Passed and Adopted on \_\_\_\_\_

Resolution No. \_\_\_\_\_



# DR. MARTIN LUTHER KING, JR. BOULEVARD

## CONTENTS

I. General Information..... 4

II. Significance ..... 6

III. Description ..... 8

IV. Planning Context .....8

V. Bibliography .....9

## I. GENERAL INFORMATION

### Historic Names:

Laborers Union Local 478

### Current Name:

Laborers Union Local 478

### Location:

799 NW62nd Street

### Present Owner:

Laborers Union Local 478  
Holding Corporation Inc.  
799 NW 62<sup>nd</sup> Street  
Miami, Florida 33150

### Present Use:

Union Labor Hall

### Zoning District:

6300 Highly Restricted Retail/ Office Building

### Tax Folio Number:

01-3114-036-0090

### Boundary Description:

#### [Building]

Lots 15 and 16 Block 1, Seventh Avenue Park Subdivision, as recorded in Plat Book 17-17 of the Public Records of Miami Dade County, Florida

#### [Parking Lot]

Lots 17 and 18, less the south 15 feet for the right-of-way, Block 1, Seventh Avenue Park Subdivision as recorded in Plat Book 17-17 of the Public Records of Miami Dade County.

### Classification:

Historic Site

## LABORERS UNION LOCAL 478

799 NW 62<sup>nd</sup> Street



location



site plan

5

### II. SIGNIFICANCE

Specific Dates: 1953

Architect: Unknown

Builder/  
Contractor: Unknown

#### Statement of Significance

Northwest Miami has historically been an area that has seen a demographic shift from once all white communities to predominantly African-American neighborhoods. While this area had been sporadically settled as a farming community in the early 20<sup>th</sup> Century, the greatest round of home building occurred in the 1920s, designed as moderately priced housing for the white middle-class. The area known as Liberty City (an area city planners now call "Model City") was a community that witnessed this demographic shift.

The influx of the African-American population to Liberty City occurred during the 1940s, following the construction of the Liberty Square Housing Project, which opened in the late 1930s. Liberty City provided relief to the extremely crowded conditions of Overtown, where most of Miami's African-Americans had settled at the beginning of the 20<sup>th</sup> century. The 1930 census counted 25,116 residents living in Overtown's 343 acres. Housing conditions for many were dismal, and accounts from the period claimed "...there are from three to fifteen shacks on a city lot of 50'x 150'. (George and Paterson, 1988:54.) These conditions, along with the white population's resistance to the expansion of the Black population to areas west or north of Overtown, led to the emergence of Liberty City as an important new residential area. (Dunn 1997:164)

The present Laborers Local Union 478 building is located in Liberty City, and the building was constructed in 1953. When it was first opened the building housed the offices of "Federal Finance Inc.," "All State Loan" and "Insured Rentals, Inc.," an automobile rental agency. The city directory lists the officers of the Federal finance company as Harold Gradsky, President, and Gilbert Hass, Vice President.

The directories list the Federal Finance company as an occupant of the building until 1962, when their name is omitted. At that time the building was occupied by Insured Rentals, Inc. and the Inter-State Acceptance Corporation. The Laborer's Local No. 478 is first listed in the 1967 edition of Polk's City Directory. They have continued to occupy the building since that time.

The Laborers' International Union was founded in 1903 as the "International Hod Carriers and Building Laborers Union." The Union was founded to combat dangerous working conditions and poor wages that plagued the industry at the turn of the century. By 1907

6



# DR. MARTIN LUTHER KING, JR. BOULEVARD

the union had 11,000 members many of whom were immigrants arriving from European countries.

By 1920 with a membership of 96,000, the union backed efforts by African-Americans to win equal treatment in all United States Unions, and denied petitions from local unions in Kansas City and Cincinnati to create segregated local chapters.

During its long history, the Laborers' International Union has maintained a policy of non-discrimination for all ethnicities. The organization's mission is implemented through the formation of nine regions, fifty-five district councils, and more than five hundred local unions.

#### Relationship to Criteria for Designation

The Laborers Local Union 478 Building has significance in the historical and architectural heritage of the City of Miami; possesses integrity of design, setting, materials, workmanship, feeling and association; and is eligible for designation under the following criteria:

3. Exemplifies the historical, cultural, political, economic, or social trends of the community.

The Laborers Local Union 478 represents the shift in the demographics of the Liberty City neighborhood, from a once almost exclusively white enclave to a community where the majority of residents are African-American. Since locating at 799 NW 62<sup>nd</sup> Avenue in 1967, the Laborers' Union has acted on behalf of the community and its residents who are employed in the construction industry.

5. Embodies those distinguishing characteristics of an architectural style, or period, or method of construction.

The Laborers Local Union 478 building hugs the northeast corner of NW 62<sup>nd</sup> Street and NW 8<sup>th</sup> Avenue. The building curves around the corner, avoiding a right angle, and then extends both north along NW 62<sup>nd</sup> Street and east along NW 8<sup>th</sup> Avenue. Although there have been some minor alterations which include the closing of window apertures and the facing of some portions with brick veneer, the building can most closely be stylistically classified as Art Moderne or Streamline Moderne.

#### III. Description

The Laborers Local Union 478 Building is representative of a style known as Art Moderne. The style was introduced in the 1930s but remained popular well into the 1960s. Art Moderne is a modern style, in that it rejects any reference to the past. Above all it emphasizes the horizontality of the building's form through the use of curved surfaces—curved end walls, curved corners, and curved bays. There is virtually no ornament on the building's surface, and the roofs are usually flat and parapeted.

The Laborers Local Union 478 building is particularly noteworthy for its curved corner entrance bay, its proximity to the street (zero foot lot line,) and the extension of the wings of the building to the north and east.

The rear (or east) side of the building creates a sharp edge, and consists of one long wall interrupted by a doorway in the approximate center. The union owns the two lots to the east which contains a surface parking lot. The parking lot has been included in the designation.

#### Alterations:

The original entry at the corner of the building facing south has been in-filled, and a newer entrance made on the east side of the building. Original plate glass windows have been sealed.

Despite these alterations, the building is deemed to have retained a significant amount of detail from its original design, and therefore maintains its physical integrity.

#### IV. PLANNING CONTEXT

##### Present Trends and Conditions:

The building at 799 NW 62<sup>nd</sup> Street has been in continuous use since its construction in 1953, and occupied by the Laborers Union Local 478 since 1967. The neighborhood has witnessed dramatic changes in the last fifty years, but the Laborers Union Local Building has maintained its original use for almost forty years.

##### Preservation Incentives:

The building, though only a single story, creates a dramatic presence along the street. The local historic designation of the building will protect it for the future.

# STREETSCAPE BEAUTIFICATION MASTER PLAN AND FACADE STANDARDS

## V. BIBLIOGRAPHY

Dunn, Marvin. *Black Miami in the Twentieth Century*. (Gainesville: University Press of Florida) 1997

George, Paul S. and Thomas K. Peterson. "Liberly Square: 1933-1987, the Origins and Evolution of a Public Housing Project." *Tequesta, The Journal of the Historical Association of Southern Florida*, Volume 48, 1988.

R.L. Polk's City Directories, 1953, 1956, 1957, 1962, 1966, 1967, 1971, 1974

Whiffen, Marcus. *American Architecture since 1780: A Guide to the Styles* Fifth Printing (Cambridge, Massachusetts: MIT Press) 1999

## ILLUSTRATIONS



Laborers Local Union 478  
South and West Elevations  
December 2004  
Janus Research



Laborers Local Union 478  
Corner Detail  
December 2004  
Janus Research



# DR. MARTIN LUTHER KING, JR. BOULEVARD



Laboreers Union Local 478  
South and East Brevortons  
December 2004  
Janus Research